



NOTICE OF THE REGULAR MEETING OF THE  
**PLANNING & ZONING COMMISSION**

March 04, 2026 at 6:00 PM

Argyle Town Hall, 308 Denton Street, Argyle, TX 76226

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**AGENDA**

Notice is hereby given of the Regular Meeting of the Planning & Zoning Commission beginning at 6:00 PM, Wednesday, March 04, 2026, at Argyle Town Hall, 308 Denton Street, Argyle, TX 76226. The Items listed below are placed on the agenda for discussion and/or action.

**COMMISSION MEMBERS AND TOWN MANAGEMENT**

**Paul Kula, Chairperson, Place 1**

**David Snell, Vice-Chair, Place 5**

**Jody Johnson, Place 2**

**Matt Nelson, Place 3**

**Marilyn Lewis, Place 4**

**Kenton Miersma, Place 6**

**Michael Udovic, Place 7**

**Harrison Wicks, Community Development Director**

**Brenda McDonald, Town Attorney**

**CALL TO ORDER**

**PLEDGE OF ALLEGIANCE**

**American Flag**

**Texas Flag: "Honor the Texas Flag; I pledge allegiance to thee Texas, one state under God, one and indivisible."**

**OPEN FORUM**

*This is an opportunity for the public to address the Commission on any matter of public business, except public hearings. Any Comments related to public hearings will be heard when the specific hearing begins. Each speaker is limited to five (5) minutes; however, time limits can be adjusted by the Chairperson. Any response from a member of the Commission is limited to a statement of specific factual information, a recitation of existing policy, or direction to staff to place the subject on the agenda for a future Planning and Zoning Commission meeting.*

**CONSENT AGENDA**

*Any Commission member may request an item on the Consent Agenda to be taken up for individual consideration.*

1. Consider approval of the minutes of the Regular Planning and Zoning Commission Meeting of February 4, 2026.
2. Consider approval of the minutes of the Special Planning and Zoning Commission Meeting of February 25, 2026.

## **STAFF REPORT**

3. Active Development Project List and Town updates.

## **PUBLIC HEARING ITEMS**

4. Hold a public hearing, consider and make a recommendation to Town Council on a Zoning Change Request (Z-25-003) from Community Retail (CR) to a Planned Development with a base zoning of Community Retail (CR) for the Heath Tract Planned Development District on a 141.67-acre tract of land, known as James P Heath Addition, Block A, Lot 1, located on the northeast corner of I-35W and FM 407 W in the Patrick Rock Survey, Abstract 1063, Town of Argyle, Denton County, Texas.
5. Hold a public hearing, consider and make a recommendation to Town Council on a Future Land Use Plan amendment (FLUP-26-001) from Estate Residential 1-acre to Mixed Use for a 10.00-acre tract of land out of the S.A. Chambers Survey, Abstract No. 308, being the northern portion of the E. W. Yeck Addition, Lot 1R-1, Town of Argyle, Denton County, Texas.
6. Hold a public hearing, consider and make a recommendation to Town Council on a Zoning Change Request (Z-26-001) from Single Family 2.5 acre Residential Estate (SF-2.5) to Local Retail (LR) for a 10.00-acre tract of land out of the S.A. Chambers Survey, Abstract No. 308, being the northern portion of the E. W. Yeck Addition, Lot 1R-1, Town of Argyle, Denton County, Texas.
7. Hold a public hearing, consider and make a recommendation to Town Council on ordinance amendment (FLUP-26-002) to the Thoroughfare Plan of the Town of Argyle Comprehensive Plan adopted June 17, 2024 as Ordinance 2024-17.

## **ACTION ITEMS**

8. Consider and make a recommendation to Town Council on Site Plan request (SP-26-001) for the proposed Akshya Foods, LLC development and restaurant use, located at 606 N US Hwy 377, being on 0.36 acres of land, known as The Original Town of Argyle, Lots 5-10, Block 8, in the J. Davis Survey, Abstract 325, Town of Argyle, Denton County, Texas.

## **WORKSHOP ITEMS**

*(Workshop items are for discussion only. No action may be taken on items listed under this portion of the agenda, other than to provide general direction to staff or to direct staff to place such items on a future agenda for action.)*

9. Discuss potential ordinance amendments and development standard updates.

## ADJOURN

**Note:** As authorized by Section 551.071 of the Texas Government Code (Consultation with Town Attorney), this meeting may be convened into closed Executive Session for the purpose of seeking confidential legal advice from the Town Attorney on any agenda item herein.

## CERTIFICATION

I hereby certify the above Notice of Meeting was posted on the bulletin board at Argyle Town Hall, a place convenient and readily accessible to the general public at all times, and to the Town's website in compliance with Chapter 551, Texas Government Code, on **February 26, 2026, by 5:00 p.m.** and remained posted for at least three (3) business days preceding the scheduled time of said meeting.

*Harrison Wicks*

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Harrison Wicks, MPA

Director of Community Development



***Persons with disabilities who plan to attend this public meeting and who may need auxiliary aid or services are requested to contact the Argyle Town Hall 48 hours in advance, at 940-464-7273, and reasonable accommodations will be made for assistance.***

**MINUTES OF THE TOWN OF ARGYLE  
PLANNING AND ZONING COMMISSION REGULAR MEETING  
WEDNESDAY, FEBRUARY 4, 2026**

A regular meeting of the Argyle Planning and Zoning Commission was held on Wednesday, February 4, 2026, at 6:00 PM. This was an OPEN MEETING, open to the public, subject to the open meeting laws of the State of Texas and, as required by law, was hereby posted on January 29, 2026, at 5:00 PM, at Argyle Town Hall, giving notice of time, date, place, and agenda thereof.

**PLANNING AND ZONING COMMISSION MEMBERS PRESENT:**

Paul Kula, Chairman  
David Snell, Vice-Chair  
Mike Udovic  
Jody Johnson  
Marilyn Lewis  
Matt Nelson

**PLANNING AND ZONING COMMISSION MEMBERS ABSENT:**

Kenton Miersma

**STAFF PRESENT:**

Mike Sims, Town Manager  
Brenda McDonald, Town Attorney  
Harrison Wicks, Director of Community Development

**REGULAR MEETING AGENDA**

**CALL TO ORDER – Chairman**

The regular meeting was called to order at 6:04 PM by Chairman Kula noting a quorum was present.

**PLEDGE OF ALLEGIANCE – American Flag**

**PLEDGE OF ALLEGIANCE – Texas Flag - “Honor the Texas Flag; I pledge allegiance to thee Texas, one state, under God, one and indivisible”.**

**OPEN FORUM:**

**Pursuant to Section 551.007 of the Texas Government Code, any member of the public has the opportunity to address the Planning and Zoning Commission concerning any matter of public business or any posted agenda item; however, the Act prohibits the Planning and Zoning Commission from deliberating any issues not on the public agenda and such non-agenda issues may be referred to Town staff for research and any future action; all persons addressing are limited to five (5) minutes and subject to council adopted rules and limitations permitted by law.**

Chairman Kula opened the meeting to public comment at 6:05 PM. With no speakers, Chairman Kula closed the open forum at 6:06 PM.

**CONSENT AGENDA**

- 1. Consider approval of the minutes of the Regular Planning and Zoning Commission Meeting of January 7, 2026.**

Commissioner Udovic motioned to approve the minutes as presented. Commissioner Johnson seconded the motion. The motion carried unanimously by a vote of six (6) in favor to none (0) opposed.

## STAFF REPORT

### 2. Active Development Project List and Town Updates.

Town staff provided the commission with recent development project updates and gave an overview of upcoming projects. No action was taken.

## PUBLIC HEARING ITEMS

### 3. Hold a public hearing, consider and make a recommendation to Town Council on a Specific Use Permit (SUP-25-003) for a Sports Court (Lighted) use on a school site owned by the Argyle Independent School District (AISD) located on the north side of FM 407 on the James P Heath Addition, Lot 1, Block A (south part), Town of Argyle, Denton County, Texas.

Director Wicks provided a presentation on the item. The Commission discussed the item and considered the specific use permit request. Town staff requested this item be tabled to a future P&Z Commission meeting.

Commissioner Nelson motioned to table Specific Use Permit (SUP-25-003) for a Sports Court (Lighted) use on a school site owned by the Argyle Independent School District (AISD), to a future P&Z Commission meeting, but not before Town staff and representatives can view the sport lights in person and activated at night. Commissioner Lewis seconded the motion. The motion carried unanimously by a vote of six (6) in favor to none (0) opposed.

### 4. Hold a public hearing, consider, and make a recommendation to Town Council on a Site Plan request (SP-25-009) with proposed variances to the Community Retail – (CR) zoning district standards, for the proposed Valley Creek Church, being on 7.45 acres of land, known as Valley Creek Church Argyle, Lot 1, Block A, in the F.W. Thorton Survey, Abstract 1244, Town of Argyle, Denton County, Texas.

Director Wicks provided a presentation on the item. The Commission discussed the item and considered the site plan request. Applicant Brad Lanham with Valley Creek Church and representative Justin Lansdowne with McAdams answered questions from the commission.

Chairman Kula opened the public hearing at 7:41 PM. With no speakers, Chairman Kula closed the public hearing at 7:41 PM.

Commissioner Udovic motioned to recommend approval to Town Council for Site Plan request (SP-25-009), with the following variances and conditions:

#### Variances

1. A variance request to the 50-foot landscape buffer requirement along Gateway Blvd to 10-foot.
2. A variance request to the 20-foot parking stall depth requirement to 18-foot.
3. A variance request to the 8-foot back-to-back landscape island width requirement for the northern most landscape islands to 6-foot back-to-back.

Conditions

A. That the applicant remove the northern most 28 parking spaces and increase the landscape buffer along FM 407 W to 28 feet in width and add a 3 foot berm in the same area.

B. That the applicant meet the Town's lighting and glare standards per Section 14.3.76

Commissioner Lewis seconded the motion. The motion carried unanimously by a vote of six (6) in favor to none (0) opposed.

**ADJOURNMENT**

Commissioner Nelson moved to adjourn the meeting. Commissioner Udovic seconded the motion. The motion carried unanimously by a vote of six (6) in favor to none (0) opposed at 7:54 PM.

**APPROVED THIS THE 4TH DAY OF MARCH 2026.**

**WITNESS:**

\_\_\_\_\_  
Paul Kula, Chairperson

\_\_\_\_\_  
Harrison Wicks, Director of Community  
Development

**MINUTES OF THE TOWN OF ARGYLE  
PLANNING AND ZONING COMMISSION SPECIAL MEETING  
WEDNESDAY, FEBRUARY 25, 2026**

A special meeting of the Argyle Planning and Zoning Commission was held on Wednesday, February 25, 2026, at 6:00 PM. This was an OPEN MEETING, open to the public, subject to the open meeting laws of the State of Texas and, as required by law, was hereby posted on February 19, 2026, at 5:00 PM, at Argyle Town Hall, giving notice of time, date, place, and agenda thereof.

**PLANNING AND ZONING COMMISSION MEMBERS PRESENT:**

Mike Udovic  
Jody Johnson  
Marilyn Lewis  
Matt Nelson

**PLANNING AND ZONING COMMISSION MEMBERS ABSENT:**

Paul Kula, Chairman  
David Snell, Vice-Chair  
Kenton Miersma

**STAFF PRESENT:**

Mike Sims, Town Manager  
Harrison Wicks, Director of Community Development  
Susan Raabe, Senior Planner

**REGULAR MEETING AGENDA**

**CALL TO ORDER – Commissioner Udovic as Chairman**

The regular meeting was called to order at 6:08 PM by acting Chairman Udovic noting a quorum was present.

**PLEDGE OF ALLEGIANCE – American Flag**

**PLEDGE OF ALLEGIANCE – Texas Flag - “Honor the Texas Flag; I pledge allegiance to thee Texas, one state, under God, one and indivisible”.**

**OPEN FORUM:**

**Pursuant to Section 551.007 of the Texas Government Code, any member of the public has the opportunity to address the Planning and Zoning Commission concerning any matter of public business or any posted agenda item; however, the Act prohibits the Planning and Zoning Commission from deliberating any issues not on the public agenda and such non-agenda issues may be referred to Town staff for research and any future action; all persons addressing are limited to five (5) minutes and subject to council adopted rules and limitations permitted by law.**

Acting Chairman Udovic opened the meeting to public comment at 6:09 PM. With no speakers, acting Chairman Udovic closed the open forum at 6:09 PM.

**WORKSHOP ITEMS**

**1. Discuss Heath Tract Planned Development application.**

Town Manager Sims presented on this item and answered questions from the Commissioners. Commissioners provided feedback for the planned development and included the following comments:

- The identified open space in the northwest corner of the development could be used for sports fields
- The landscape buffer along FM 407 should include a 3 foot earthen berm, similar to other projects in this FM 407 corridor.
- Area E on the concept plan should be preserved as open space only. No uses should be listed as permitted in Area E.

**2. Discuss Town Center District Plan.**

Town Manager Sims presented on this item and answered questions from the Commissioners. Commissioners provided feedback for the small area plan and included the following comments:

- There was concern on the proposed density of the 18 acres on the east side of the Waterbrook neighborhood, on the Town-owned 6 acres and 12 acres owned by a private property owner.
- There was concern on the type of access to the Waterbrook commercial area from the north side of FM 407, south to connect the two portions of the Town Center District. Pedestrian access would be challenging across FM 407.
- There was a question on the limits of the small area plan. What if the plan was reduced in size to have its eastern boundary be Blue Horizon Way, instead of Myrtle Drive as it currently is shown.

There was a consensus that the small area plan is beneficial and should move forward with review and finalization.

**ADJOURNMENT**

Commissioner Lewis moved to adjourn the meeting. Commissioner Nelson seconded the motion. The motion carried unanimously by a vote of four (4) in favor to none (0) opposed at 7:51 PM.

**APPROVED THIS THE 4TH DAY OF MARCH 2026.**

**WITNESS:**

\_\_\_\_\_  
Paul Kula, Chairperson

\_\_\_\_\_  
Harrison Wicks, Director of Community  
Development

## Active DRC Project List - 3.4.2026

SI	Project #	Project Name	Type/Location	Status	Comments
1	SUP-25-003	AISD Middle School Light Sports Court Use	Specific Use Permit	Scheduled for Future PZ Meeting, Must see lights on at night first	Specific Use Permit (SUP-25-003) for a Sports Court (Lighted) on a school site owned by the Argyle Independent School District (AISD) located on the north side of FM 407 on the James P Heath Addition, Lot 1, Block A (south part), Town of Argyle, Denton County, Texas.
2	Z-26-001 & FLUP-26-001	Town owned 6-acre Property on FM 407, plus 4 acres of property to the south	Zoning Change and Future Land Use Amendment	Scheduled for PZ 3.4.26	Staff is working with the property owner of the 12 acres to the south of the Town owned property on the zoning change and future land use plan amendment. These items were presented during a community town hall meeting with residents of the Waterbrook and Avalon neighborhoods on February 24th and during a special P&Z Commission meeting on February 25th. These items will now have a public hearing during the regular PZ Commission meeting on March 4, 2026.
3	Z-25-003	Heath Tract	Planned Development	Scheduled for PZ 3.4.26	Staff provided a presentation on this item during the special P&Z Commission meeting on February 25th. A representative of the applicant attended the meeting and listened to the valuable feedback from the commissioners. This item will now have a public hearing during the regular PZ Commission meeting on March 4, 2026.
4	FLUP-26-002	Thoroughfare Plan Update	Comp Plan Amendment	Scheduled for PZ 3.4.26	Staff plans to present modifications to the Thoroughfare Plan to the Comp Plan Advisory Committee during their meeting on Monday, March 2nd. Staff will then present this item and hold a public hearing during the regular PZ Commission meeting on March 4, 2026. The changes to the Thoroughfare Plan were to conform with the small area plans currently underway, as well as to add roads and traffic calming measures that could be covered under roadway impact fees in the future.
5	SP-26-001	Akshya Foods - Food Truck	Site Plan	Scheduled for PZ 3.4.26	Staff received a site plan application for a restaurant use at 606 N US Hwy 377, Argyle, TX 76226. The property owner is proposing to use a food truck on site to serve patrons and have seating inside the main structure and outside on a patio space. The site plan proposes to construct new parking to serve the property, with a landscape buffer along US Hwy 377.
6	N/A	Argyle Sports Academy	Construction Plans	Town Staff Review	Staff is continuing to review the construction plan submittal for the Argyle Sports Academy located on a portion of Lot 92, Block D in Argyle Landing, located in the F.W. Thornton Survey, Abstract Number 1244, Town of Argyle, Denton County, Texas.
7	N/A	Marsden Tract - Argyle Marketplace	Construction Plans	Town Staff Review	Staff is continuing to review the construction plan submittal for the Marsden Tract - Argyle Marketplace development, located on a 1.97-acre tract of unplatted land, known as A0308A S.A. Chambers, Tract 20, addressed as 409 US Hwy 377 South, Town of Argyle, Denton County, Texas. Town staff is also working on the building relocation of the train depot to Town property on Front Street. 3rd party contractor, Denton County Housemovers, is preping the building now for future transport.
8	N/A	Knights Ridge	Construction Plans	Town Staff Review	Staff is continuing to review the construction plan submittal for Knights Ridge, Lots 1-25, 26X & 27X Block 1, being 72.126 acres situated in the T&P RR Co survey abstract no. 1293 Town of Argyle Denton County, Texas. Town staff also finalized the LOMR package that the applicant will now submit to FEMA.



## PLANNING AND ZONING COMMISSION STAFF REPORT

**Meeting**

**Date:** March 4, 2026

**To:** Chairman and Members of the Planning and Zoning Commission

**From:** Harrison Wicks, Director of Community Development

**Subject:** Public Hearing - Heath Tract Planned Development

**Purpose:**

Hold a public hearing, consider and make a recommendation to Town Council on a Zoning Change Request (Z-25-003) from Community Retail (CR) to a Planned Development with a base zoning of Community Retail (CR) for the Heath Tract Planned Development District on a 141.67-acre tract of land, known as James P Heath Addition, Block A, Lot 1, located on the northeast corner of I-35W and FM 407 W in the Patrick Rock Survey, Abstract 1063, Town of Argyle, Denton County, Texas.

**Existing Condition of Property:**

The property is zoned Community Retail (CR), and fronts on FM 407 and Gateway Blvd. The site is currently an unimproved open pasture.

**Adjacent Existing Land Uses and Zoning:**

North: Sam Davis Road Right-of-Way  
 South: FM 407 Right-of-Way  
 East: AISD Elementary and Middle Schools  
 West: TXDOT Right-of-Way, Sam Davis Road Right-of-Way

**Background:**

The current property owner, Argyle Commercial 94 Land LLC, has owned the property since March 2022. A previous meeting includes a Program for Argyle Community Engagement (PACE) meeting with the property owner back in August 2022. This is their first formal zoning application submittal.

**Development Review Analysis:**

The applicant, Spiars Engineering, on behalf of the owner, Argyle Commercial 94 Land LLC, has requested a zoning change for the creation of a Planned Development zoning district, known as the Heath Tract Planned Development, on approximately 142 acres of the northeast corner of I35W and FM 407.

The plans depict a retail, hotel, and office development with no residential uses proposed. The base zoning

district for the PD is Community Retail (CR) with specific deviations/enhancements provided by the applicant.

**Consistency with the Future Land Use Plan and Comprehensive Plan:**

Town staff have reviewed the Zoning Change Application and have determined that the applicant has demonstrated compliance with the Future Land Use Plan (FLUP). This area is identified as a commercial district on the FLUP, which is characterized by high intensity commercial with large retail anchors, smaller in-fill commercial, and ample parking to accommodate regionally generated traffic. No residential uses are permitted within this district. These uses require high visibility locations and should only be located on major roadways and corridors.

**Area Regulations:**

The PD proposes to use the area regulations of the CR Zoning District. The CR Zoning District has a minimum front yard setback of twenty-five feet (25'), side yard setbacks of twenty feet (20') and rear yard setbacks of twenty feet (20'). There is no minimum lot area requirement.

**Parking:**

For purposes of the PD, minimum parking spaces shall adhere to Sec. 14.3.66, - Off Street Parking and Loading Requirements in the Town of Argyle Development Standards. Alternative standards in the PD include, that the Community Development Director may authorize a reduction in parking up to 12% of the requirement where requested by the applicant, and required parking for a General Retail Large Scale use shall be one (1) space per 400 square feet of floor area, where previously the requirement in Sec. 14.3.66 is one (1) space per 250 square feet of floor area.

**Landscaping:**

Site Landscaping shall comply with the provisions of Sec. 14.3.67-6 – Minimum Landscaping Requirements For Nonresidential and Multifamily Developments. Landscape coverage required for individual sites in the PD is 25%. The project is required to provide a Type D landscape buffer along the front property line along FM 407. The buffer width is 10 feet. The applicant has proposed to increase the width of the buffer along FM 407 to a Type E bufferyard with 50 feet, and 4 canopy and 8 understory trees every 100 linear feet.

Along the entire east side property line, adjacent to the Grove Church and AISD property, the applicant has proposed no change to the Town standard of a Type F bufferyard with 50 feet, and 4 canopy, 8 understory trees, and 12 shrubs every 100 linear feet. This also includes an 8-foot-high masonry wall.

Along the entire north side property line, adjacent to Sam Davis Rd, the applicant has proposed a Type E bufferyard with 50 feet, and 4 canopy and 8 understory trees every 100 linear feet. The requirement of 12 shrubs per 100 linear feet was not included.

Along the entire west side property line, adjacent to Sam Davis Rd and I35W, the applicant has proposed a Type B bufferyard with 10 feet, and 2 canopy and 3 understory trees, and 10 shrubs every 100 linear feet. The Town standard originally called for a Type D bufferyard in this area with 10 feet, and 4 canopy and 5 understory trees, and 18 shrubs every 100 linear feet.

**Design Standards:**

The applicant has submitted an exhibit showing character imagery for the PD. The building elevations show 80-90% masonry materials, with flat roofs and parapets, and multiple articulation techniques to break up the building façade. The CR Zoning district requires for every 80 feet (80') of a wall visible from a public street or a residentially zoned property, there be a change in the building footprint measuring a minimum of three feet (3'). The applicant has provided the appropriate footprint offsets.

**Signage:**

The applicant intends to comply with the provisions of Article IV. — Sign Regulations of the Town of Argyle Development Standards for design, construction, and maintenance, with the exception of wayfinding signage and district or neighborhood signs.

**Uses:**

The following uses were added to the PD in order to allow them as permitted in select areas. These areas are identified as along FM 407 only:

**Hotel (Permitted in Areas A, B)** - A Full-Service Hotel use is a facility providing temporary lodging or guest rooms to the general public on a daily-rate basis, which must include no fewer than 90 rooms accessible by interior corridors, indoor pool, and must also include a bar and full-service menu-based breakfast. Accessory services such as meeting or conference rooms, housekeeping, and recreational amenities may be included. Hotel shall not exceed a maximum height of sixty (60) feet or four (4) stories.

**Warehouse Club (Permitted in Area C)** - A Warehouse Club use is a membership-based warehouse retail use that sells a limited variety of general merchandise and food in bulk quantities to the general public. Products may include foodstuffs, apparel, furniture, appliances, home furnishings, toiletries, cosmetics, photographic equipment, electronics, toys, and other general merchandise. This use requires customers to pay a membership fee to access the exclusive, discounted, bulk-packaged goods. This use is exempt from the Development, Large Scale use definition and requirement of a specific use permit if the building is larger than 40,000 square feet. There is no minimum or maximum square footage for this use.

**Traffic Impact Analysis:**

The Development is proposed to consist of restaurant, hotel, and other retail uses and is expected to be ready for occupancy by 2035. The development proposes to have access along the future I-35W Northbound Frontage Road, FM 407 and the future extension of Gateway Blvd.

Based on the Town of Argyle's Development Standards Section 14.2.90.C.6 stating that the minimum acceptable congestion level of Level of Service (LOS) C, the following principal findings were found as part of Spiars' analyses of projected operational conditions at the "Build" condition.

***Project Background Findings***

- Roadway Improvements to I-35W and FM 407 within the project buildout were made known to Spiars at the onset of the traffic study. I-35W will be reconfigured from a 4-lane rural interstate to a six-lane urban freeway with an off-ramp connection to the northbound frontage road just north of FM 407. The interchange with FM 407 will also include a U-turn lane for the southbound frontage road in connection with the northbound frontage road.

Additionally, FM 407 will be reconstructed from a 2-lane highway to a four-lane median-divided highway from I-35W to Gateway Blvd. These assumptions have been applied to the analysis in the year 2030 and 2035 scenarios.

- In addition to the above improvements, the Town will install traffic signals at the intersections of FM 407 at Avalon Boulevard and FM 407 at Gateway Drive. For purposes of this study, traffic signals have been assumed at these intersections for the No-Build and Build Conditions.

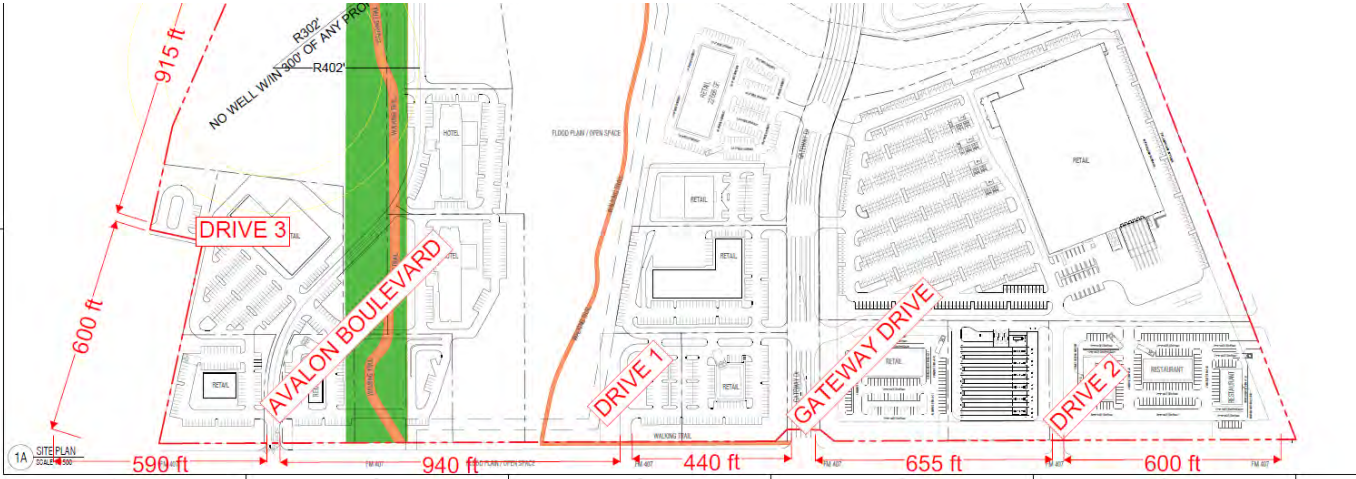
**Level of Service Findings**

- All intersections and roadways (FM 407) within the study area operate at “acceptable” overall intersection Levels of Services during the Existing Conditions. With the addition of estimated background traffic growth and estimated site development traffic, the overall intersection Levels of Service are projected to continue to operate at “acceptable” Levels of Service with slight increases to the delays except for the following intersections.

- The intersection of I-35W and the Southbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below.

- The intersection of I-35W and the Northbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below. Mitigations for the degrading conditions are listed in the table below.

- The exiting southbound maneuvers at Drive 2 along FM 407 are projected to operate at “unacceptable” Levels of Service in the project Build Conditions in both the AM and PM Peak Hours. These “unacceptable” conditions are typical of other similar unsignalized intersections and driveways along major high-volume roadways such as FM 407 and can only be mitigated with the installation of a traffic signal. However, with the anticipated low traffic volumes exiting the minor approach at the driveway from the development, the warrants for a traffic signal are estimated to not be met. Therefore, no mitigations for the “unacceptable” Levels of Service are recommended.



Traffic Impact Analysis Mitigations Table

Scenario	Year	Intersection/Approach/Peak	Recommendation	Responsible Party	Pro-Rata
Build	2035	FM 407 and I-35W Southbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2035	FM 407 and I-35W Northbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2028	FM 407 and Avalon Boulevard	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Drive 2	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Westbound Right-Turn Lane	Development	100%

**Attachments:**

- Exhibit A – District Standards
- Exhibit C – Concept Plan
- Exhibit D – Illustrative Plan
- Exhibit E – Phasing Plan
- Exhibit F – Open Space Exhibit
- Exhibit G – Paving Sections
- Exhibit H – Trail Exhibit
- Exhibit J – Landscape and Screening Character Images
- Traffic Impact Analysis

# EXHIBIT A

## HEATH TRACT PLANNED DEVELOPMENT DISTRICT

### TABLE OF CONTENTS

- Section 1. Administration
- Section 2. Development Approvals Processes
- Section 3. Site Development Standards
- Section 4. Lot and Building Standards
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- Section 6. LandscapeStandards
- Section 7. Signage Standards
- Section 8. Permitted Uses
- Section 9. Definitions

### SECTION 1. ADMINISTRATION

- 1.1. Title  
This Planned Development Zoning District is known as the "Heath Tract Planned Development District". This area and the ordinance establishing applicable regulations and may be cited as the "District".
- 1.2. Purpose  
The Purpose of this Planned Development Zoning District is to establish minimum requirements of development standards for a mix of uses. Following approval of this ordinance the developer shall prepare, or have prepared on their behalf, in a phased manner where applicable a Site Plan for each area to be developed and conforming to the criteria established herein.
- 1.3. Applicability
  - 1.3.1. Planned Development Zoning District Applicability  
The provisions of this District shall only apply to properties as indicated on Exhibit A: Legal Description as further identified in Exhibit B: Zoning Exhibit. This Planned Development Zoning District provides regulations including zoning, subdivision, signage, and design standards for land designated as "Heath Tract Planned Development District".

1.3.2. Relationship to the Town of Argyle Code of Ordinances

- 1.3.2.1. In the event of a conflict between the provisions of this District and any other codes, ordinances, regulations, and standards of the Town of Argyle, the provisions of this District shall control.
- 1.3.2.2. Provisions of any other codes, ordinances, regulations, and standards of the Town of Argyle that are not in conflict with the provisions of this District shall continue to apply to the property designated as the District.
- 1.3.2.3. The definitions set forth herein contain terms that are integral to this District. Those terms not defined in Section 9 or in Sec. 14.1.5 - Definitions of the Town of Argyle Development Standards; shall be accorded their commonly accepted meanings. In the event of a conflict between a definition in this District and a definition in any other codes, ordinances, regulations, and standards of the Town of Argyle, the definition in this District shall control.

1.3.3. Interpretation

- 1.3.3.1. In the event of a conflict between the provisions of this District and the numerical metrics of its tables and the diagrams and illustrations, the provisions of this District shall control.
- 1.3.3.2. Provisions of this District are activated by "SHALL" or "ARE" when required and "MAY" when optional.

SECTION 2. DEVELOPMENT APPROVALS PROCESSES

2.1. Development Procedure

The development procedure shall conform to the standards and progression as set forth in "Sec. 14.3.25. —Concept Plan and Site Plan Review Processes." in the Town of Argyle Development Standards, unless amended herein. The specific process shall depend upon the type of application as described below.

2.1.1. Concept Plan

The Concept Plan (included as Exhibit C: Concept Plan) for the PD shall satisfy the requirements for the Concept Plan as described in Sec. 14.3.25.5 and the requirements for a Phase One Concept Plan as described in Sec. 14.3.51-5. — "Large Commercial/Mixed-Use Development PD" in the Town of Argyle Development Standards and as modified herein. Council approval of this Planned Development District by Ordinance shall constitute approval of the

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Concept Plan allowing for an Applicant to proceed to a Site Plan. Revisions to the Concept Plan, either in whole or in part, shall be subject to the Major and Minor Amendments in Section 2.2.1.

The concept plan is provided for preliminary planning purposes only and is not an official or submitted document. All aerial imagery and mapping are based on the best available information at the time of preparation. The plan is subject to revision.

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2.1.2. Site Plan

A Site Plan that is in general conformance with an approved Concept Plan shall be reviewed in accordance with the process described in "Sec. 14.3.25.-2" in the Town of Argyle Development Standards. A Site Plan that is in general conformance with the Concept Plan and all other applicable requirements of the Development Standards shall be approved or approved with conditions.

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The site plan is provided for preliminary planning purposes only and is not an official or submitted document. All aerial imagery and mapping are based on the best available information at the time of preparation. The plan is subject to revision.

2.2. Flexibility

The phasing and complexity inherent to mixed-use developments and the necessary extended timeframe to complete the development require that flexibility be incorporated in the entitlements for the District. There shall be two types of flexibility from the requirements of this District: Major Amendments and Minor Amendments.

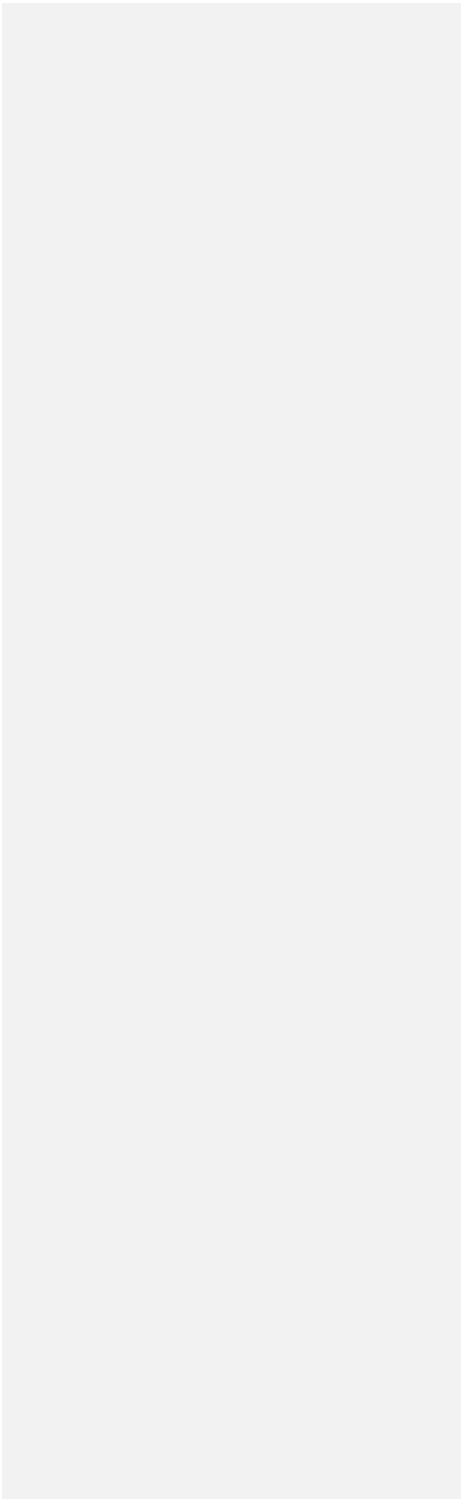
2.2.1. Major and Minor Amendments

Major and Minor Amendments to the approved Concept Plan shall follow the criteria and procedures below and shall be determined by the Community Development Director.

Table 2.2.1: Major and Minor Amendments

Minor Amendment	Major Amendment
Modification up to 10% to the numerical standards of the district	Modification greater than 10% to the numerical standards of the district
Throughfares and Blocks on a Concept Plan may be modified to while maintaining general throughfare connectivity and spacing requirements. Resulting changes in blocks due to the shifting of throughfares shall not cause more than a 25% change in the size of the block.	Any change to a Concept Plan in excess of the Minor Amendment threshold of modifications.
Modification of the anticipated order of development for the overall property	

Modifications to approved, Street Cross Sections, and Intersection Assemblies to accommodate specific considerations regarding traffic calming, emergency vehicle access, accommodate pedestrian	
--	--



and bike traffic, and modify on-street parking configuration	
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2.2.2. Administrative Modification  
 Minor Amendments shall be approved administratively by the Town Administrator or his / her designee. Major Amendments shall require the recommendation of the Planning and Zoning Commission and approval of Town Council.

**SECTION 3. SITE DEVELOPMENT STANDARDS**

3.1. Areas  
 The District is comprised of five primary Areas: A, B, C, D, and E. Areas are to be located as depicted on the Concept Plan. Areas within this District shall comply with their corresponding base zoning districts and permissible use standards, as set forth in Argyle’s Development Standards, except as provided herein.

Areas and their base zoning district designations are the following:

Table 3.1.1: Area Base Zoning

Area	Base Zoning District
A	Community Retail (CR)
B	Community Retail (CR)
C	Community Retail (CR)
D	Community Retail (CR)
E	Community Retail (CR)

3.2. Thoroughfare Standards  
 All Thoroughfares shall be constructed in accordance with the standards as shown in Exhibit D: Thoroughfare Plan and Exhibit E: Street Sections.

3.2.1. Thoroughfare Modifications  
 Modifications to approved Thoroughfares, Street Cross Sections, and Intersection Assemblies to accommodate specific considerations regarding traffic calming, accommodate pedestrian and bike traffic, and modify on-street parking configuration may be approved by Minor Amendment by the Community Development Director. The Boulevard Street Section as depicted in Exhibit E: Street Sections may be built in phases — allowing just one side of the boulevard to serve as an access street.

3.2.2. Traffic Calming

- 3.2.2.1. Medians  
 Medians are permitted to be constructed in Gateway Drive.
- 3.2.2.2. Bulb-outs  
 Bulb-outs or similar type curb extensions may be used along key

intersections and programmed open space where feasible to support pedestrian safety and where vehicular movement and emergency access will not be unnecessarily hindered.

#### 3.2.2.3. Speed Tables

Speed Tables may be permitted with administrative approval. Speed Tables may be integrated into pedestrian crossings at intersections and may be used to integrate Open Spaces across a street.

### 3.3. Open Space Standards

3.3.1 Open Space Requirements – An 8’ trail shall be provided along the east side of the floodplain.

3.3.2 All open space shall be owned and maintained by the Property Owners’ s Association ( POA).

3.3.3 All trails within the area designated as open space shall be open to the public and constructed of decomposed crushed granite or concrete.

3.3.4 The development shall provide a minimum of 26(as shown per the exhibit) acres of open space located along the floodplain corridor, enhanced with amenities such as landscaping, seating areas, picnic tables, and pedestrian trails. The planned trail system will be designed to connect with the Town of Argyle’s comprehensive trail network south of FM 407. The proposed open space exceeds the Town’s minimum open space requirements. One (1) amenity item for every acre of open space shall be provided. Amenity items may include the following:

- o Fountains
- o Water features
- o Outdoor seating
- o Public art
- o Educational signage
- o Bike racks
- o Exercise equipment (parcourse);
- o Shade structure;
- o Gazebo;
- o Signature lighting;
- o Sport courts/ fields;
- o Open air picnic lawn;
- o Playground;
- o Dog Park facilities;
- o Other amenities approved by the Community Development Director.
- o
- o Other amenities approved by the Community Development Director.

3.3.5 The Open Space standards of this planned development shall satisfy all Town of Argyle Parks dedication requirements, and all Open Space requirements current or in the future.

3.3.6 Open Space shown on the **Open Space Plan** shall count towards the required landscaping percentage on the lot on which it is located.

3.3.7 The Open Space standards contained in this planned development shall be the sole standards for Open Space for development within the Property, including the standards for Large-Scale Projects / Developments.

### 3.4. Site Design

#### 3.4.1. Commercial Shared Access

- Shared access and / or access easements across commercial parcels are permitted. The locations of shared access shall be indicated on the Site Plan.
- Commercial development shall be designed to provide shared access to contiguous commercial parcels provided the contiguous parcel has provided for the extension of the shared access.
- Provisions shall be made for the connection of pedestrian and vehicle circulation systems with contiguous parcels.
- Vehicular access easements from one lot to contiguous lots and for private driveways within a lot may be provided on the final plat or by separate recorded instrument.

## SECTION 4. LOT AND BUILDING STANDARDS

### 4.1 Building Standards

#### 4.1.1 Building Height

- 4.1.1.1 Masts, water towers, belfries, clock towers, chimney flues, mechanical equipment, cooling towers, air conditioning equipment, visual screens which surround roof mounted mechanical equipment, parapet walls limited to four feet in height, cupolas, spires, steeples, turrets, elevator bulkheads and similar building elements shall not count towards maximum building height.
- 4.1.1.2 Communication antennas and support structures *may* exceed building height by 12 feet.
- 4.1.1.3 Building wall shall extend to parapets that enclose the roof area for flat roofs. Said parapets shall be of a sufficient height to fully screen the roof and any mechanical equipment located on the roof.

#### 4.1.2 Building Standards

- 4.1.2.1 Buildings that are open to the public and are within 30 feet of the street right-of-way shall have a pedestrian walkway shall consist of a sidewalk meeting ADA criteria. The sidewalk shall be level with the pavement.
- 4.1.2.2 Loading docks or loading areas are permitted to be visible from the street and may be accessed directly from the street.
- 4.1.2.3 Outside display and sales of merchandise is allowed.
- 4.1.2.4 Outside storage of merchandise may remain on a lot for more than 24 hours.

### 4.2 Lot Standards

#### 4.2.1 Frontage Lot widths

- 4.2.1.1 All lots abutting or fronting a designated major thoroughfare with shared access shall maintain a minimum lot width of one hundred twenty-five (125) feet.

## SECTION 5. PARKING STANDARDS

### 5.1 General

The parking requirements shall be determined by the use, as provided below. The parking provided shall include the parking spaces provided within the lot and the parking spaces that are along the parking lane corresponding to lot frontages.

#### 5.2 Parking Requirements

For purposes of the District, minimum parking spaces shall adhere to Sec. 14.3.66, - Off-Street Parking and Loading Requirements in the Town of Argyle Development Standards. The Community Development Director may authorize a reduction in parking up to 12% of the requirement where requested by the applicant.

- Required parking may be provided with shared parking agreements among property owners, tenants, or users.
- Required parking for a General Retail Large Scale shall be one (1) space per 400 square feet of floor area

#### 5.3 Parking Lot Landscaping

- Parking lots shall comply with the "Minimum Requirements for Off-Street Parking and Vehicular Use Areas" found in Sec.14.3.67-6. K— "Minimum Landscaping Requirements for Nonresidential and Multifamily Developments".
- Parking lots shall be accessed by a minimum six-foot-wide sidewalk.
- Trees shall not be required to be planted in landscape islands being within 300 feet of a grocery store front entrance(s)

### SECTION 6. LANDSCAPE STANDARDS

#### 6.1 Landscape Standards

All landscaping shall comply with the provisions set forth herein. Developers shall select from Sec. 14.5.50. - Approved Plant List of the Town of Argyle Development Standards

#### 6.2 Street Trees

##### 6.2.1 Street Tree Requirement

Street Trees (minimum 4-inch caliper) shall be planted on-center, approximately every 30 feet in the planting strip of throughfares.

##### 6.2.2 Tree Species

Street Tree species shall be indicated on a Site Plan at time of Development, that demonstrates a variety of street tree species across the site to maintain a mix of tree species to ensure structural and aesthetic variety, and resistance to disease and pests over a monoculture.

##### 6.2.3 Planting Location

Flexibility on planting is permitted to accommodate public and private infrastructure. Street Trees do not require root barrier (if planted at least three feet from the back of curb) or structural soil.

#### 6.3 Site Landscaping

##### 6.3.1 Minimum Site Landscaping

Site Landscaping shall comply with the provisions of Sec. 14.3.67-6, "minimum Landscaping Requirements For Nonresidential and Multifamily Developments" of the Town of Argyle Development Standards with the following exceptions:

## SECTION 7. SIGNAGE STANDARDS

### 7.1 General Standards

- Signage may only be externally lit with full-spectrum source, unless otherwise indicated herein or with administrative approval, subject to review by the Director of Community Development
- Direct lighting, back lighting, and halo lighting is permitted
- One address number will be attached to the building in proximity to the principal entrance, and one address number shall be installed over the garage or the rear entrance of a building. Placement and size shall comply with the local fire code.
- All signs shall comply with the provisions of Article IV. — Sign Regulations of the Town of Argyle Development Standards for design, construction, and maintenance, except as provided below.
- The Development may include a pylon sign up to 35 feet in height along Interstate 35, located within the approved sign area. A coordinated sign package for the development will accompany the Site Plan submission.

### 7.2 Sign Types

#### 7.2.1 Wayfinding Signs

- Wayfinding signs will be uniform in material and color to create a sense of place in the development.
- Wayfinding signs are permitted in the right-of-way.
- Wayfinding signs may be freestanding or attached to building walls or structures.
- They may be illuminated and/or non-illuminated.
- Wayfinding signs may allow a business logo.

#### 7.2.2 Sales, Leasing and Construction Related Signs

Offsite sales, leasing and construction related signs are permitted by right during a construction period. All other sales, leasing and construction-related signs shall be approved with administrative approval, subject to review by the Community Development Director. Offsite sales, leasing and construction-related signs shall be removed no later than 14 days following the completion of construction or fulfillment of the advertised sale or lease.

#### 7.2.3 Temporary Signs

Temporary signs are allowed by right during the construction period. In all other cases they shall comply with all the applicable regulations and restrictions found in the Town of Argyle Development Standards.

#### 7.2.4 District or Neighborhood Signs

These signs will be uniform in material and color to create a sense of place in the district and shall be installed with administrative approval, subject to review by the Community Development Director.

## SECTION 8. PERMITTED USES

### 8.1 General

Permitted uses in Areas shall be in accordance with uses and activities listed for their

corresponding base zoning districts in Sec. 14.3.62 - Use Chart Matrix in the Town of Argyle Development Standards.

Legend for Use Modifications		Area A Base Zoning: CR	Area B Base Zoning: CR	Area C Base Zoning: CR	Area D Base Zoning: CR	Area E Base Zoning: CR
P	Permitted in Area					
No.	Use					
	Carwash, Self-Service (only as an accessory use to a grocery)	P		P		
33	Clinic (Medical)	P	P	P	P	P
42	Concrete or Asphalt Batching Plant (Temporary)				P	P
45	Convenience Store with Gas Sales	P				
50	Data Center				S	
54	Development, Large Scale	<u>PSUP</u>	<u>PSUP</u>	<u>PSUP</u>	<u>P</u>	
174	Urgent Care Clinic	P	P	P	P	P
	Restaurant with Drive Thru, not stacked next to each other	P	P	P		
	Hotel :	P	P	S	S	
	Office	P	P	P	P	P
			P		P	
	Motor Vehicle Repair and Services, Minor	P	P	<u>P</u>		
	Special Event Venue	P	P		P	
	<u>Warehouse Club</u>			<u>P</u>		

Prohibited Uses in this planned development from the base Community Retail (CR) zoning district include the following:

- Antennas
- Check Cashing Service
- Consignment Shop
- General Machine Repair
- Motel
- Group Day Care Home

- Helistop
- Kiosk
- Laundry Mat
- Recycling Kiosk
- Sign Manufacturing
- Tattoo Studio
- Veterinarian (Outdoor Kennels)
- Wind Energy Systems

#### SECTION 9. DEFINITIONS

This District provides definitions for terms in this District that are technical in nature or that otherwise may not reflect a common usage of the term. If a term is not defined in this District, then the Development Standards definitions shall control.

**ADMINISTRATIVE APPROVAL:** An approval by a City Staff member of a matter permitted in this District that complies with the intent and purposes of this District.

**ALLOWED ENCROACHMENTS:** Any structural element that encroaches into a Frontage, setback or easements. Allowed encroachments include eaves, gutters, chimneys, utility meters, planter boxes, porches, stoops, bay windows, balconies, terraces, oriel, and colonnades.

**AMBULANCE SERVICE:** Service that provides emergency medical care and transports patients to hospitals or other treatment facilities

**ANMBULATORY SURGERY CENTER:** facility that provides surgical care to patients who do not require hospitalization

**AMENITIZED RETENTION AREA:** Retention or detention that is programmed with amenities and designed such that its storm management function is not the dominant design characteristic

**BLOCK FACE:** The aggregate of all the building facades on one side of a block.

**BUILDING CONFIGURATION:** The form of a building, based on its massing, on its private frontage, and on its height.

**BUILDING DISPOSITION:** The placement of a building on its lot.

**BUILDING HEIGHT:** The vertical extent of a building measured in stories.

**BUILDING TYPE:** A structure category determined by function, position on the lot, and configuration, including frontage and height.

**BY RIGHT:** A use or development that complies with this District and is permitted administratively without special review or approval by the Town Council or any board or commission. (SEE MINOR ~~ADMENDMENT~~ **AMENDMENT**).

**COMMERCIAL:** The term collectively defines lodging, office, restaurant, and retail uses.

**CONCEPT PLAN:** For purposes of this zoning district, provisions which reference the Concept Plan shall be construed as meaning Exhibit B (labelled "Concept Plan"). For all clauses which refer to a concept plan, such provisions ~~shall~~ **shall** be deemed to refer to all those materials (as may be amended), in addition to any other approved documents which constitute a required part of a concept plan under the Development Standards.

**ELEVATION:** An exterior wall of a building that is not along a frontage line (SEE FAÇADE).

**ENCROACH:** To break the plane of either a vertical or horizontal regulatory limit with a structural element, so that it extends into a setback, into the public frontage,

or above a height limit.

FACADE: The exterior wall of a building set along a frontage line.

FRONT SETBACK: The distance as measured from the frontage line to the point where a building may be constructed. This area shall be maintained clear of permanent structures with the exception of Allowed Encroachments.

FRONTAGE: The area between a building facade and the vehicular lanes and is inclusive of its built and its planted components. A frontage is divided into the private frontage and the public frontage.

FRONTAGE LINE: A lot line bordering a public frontage.

FULL SERVICE GENERAL ACUTE HOSPITAL: a facility that offers services, facilities, *and* beds for use for more than 24 hours for individuals requiring care; and regularly maintains, at a minimum, clinical laboratory services, diagnostic X-ray services, treatment facilities including surgery or obstetrical care or both, and other definitive medical or surgical treatment of similar extent.

GREEN: An open space for unstructured recreation, and that is spatially defined by landscaping rather than building frontages.

HOTEL: ~~A facility providing temporary lodging or guest rooms to the general public on a daily-rate basis, which must include no fewer than 90 rooms, indoor pool, may also include accessory services such as restaurants, meeting or conference rooms, housekeeping, and recreational amenities. Hotel shall have direct pedestrian access to the trail system if the trail system is on west side of the creek. Hotels shall not exceed a maximum height of sixty (60) feet or \_\_\_\_\_ four \_\_\_\_\_ (4) \_\_\_\_\_ stories."~~

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A Full Service Hotel use is a facility providing temporary lodging or guest rooms to the general public on a daily-rate basis, which must include no fewer than 90 rooms accessible by interior corridors, indoor pool, and must also include a bar and full-service menu-based breakfast. Accessory services such as meeting or conference rooms, housekeeping, and recreational amenities may be included. Hotel shall not exceed a maximum height of sixty (60) feet or four (4) stories.

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LABORATORY, MEDICAL OR DENTAL: facility that tests clinical specimens to help diagnose or treat patients and/or a facility that designs, fabricates, or repairs dental appliances.

LANE MARKER SIGN: Identification signage that includes designated parking or curbside pickup signs

DEVELOPMENT, LARGE SCALE: a building, regardless of use, that is 40,000 square feet or larger.

MURAL: Any piece of artwork that is painted, tiled, or otherwise applied directly to an exterior wall.

ORDER CANOPY: Structure that covers drive-thru signage

PRINCIPAL BUILDING: The main building on a lot, and usually located toward the frontage.

PRINCIPAL ENTRANCE: The main point of access for pedestrians into a building.

REAR YARD: A disposition where the building occupies the full frontage, leaving the rear of the lot as the sole yard.

REAR SETBACK: The distance as measured from the rear lot line to the point where a building may be constructed. This area shall be maintained clear of permanent structures with the exception of encroachments.

SIDE YARD: A disposition where the building occupies one side of the lot with the setback to the other side. If the adjacent building is similar, with a blank side wall, the yard is quite private. May include exclusive-use and maintenance easements.

THOROUGHFARE: A public way for use by vehicular and pedestrian traffic and intended to provide access to lots and open spaces, consisting of vehicular lanes and the public frontage.

TRAFFIC CALMING: A method for using either horizontal deflection methods such as bump outs or chicanes or vertical deflection such as speed tables to slow traffic.

TRANSITIONAL OR OCCUPATIONAL THERAPY LICENSED AS ASSISTED LIVING: Facility that offers beds for use for more than 24 hours for individuals requiring treatment, or care for injury, to help residents maintain or improve their ability to perform daily activity, limited to 12 beds.

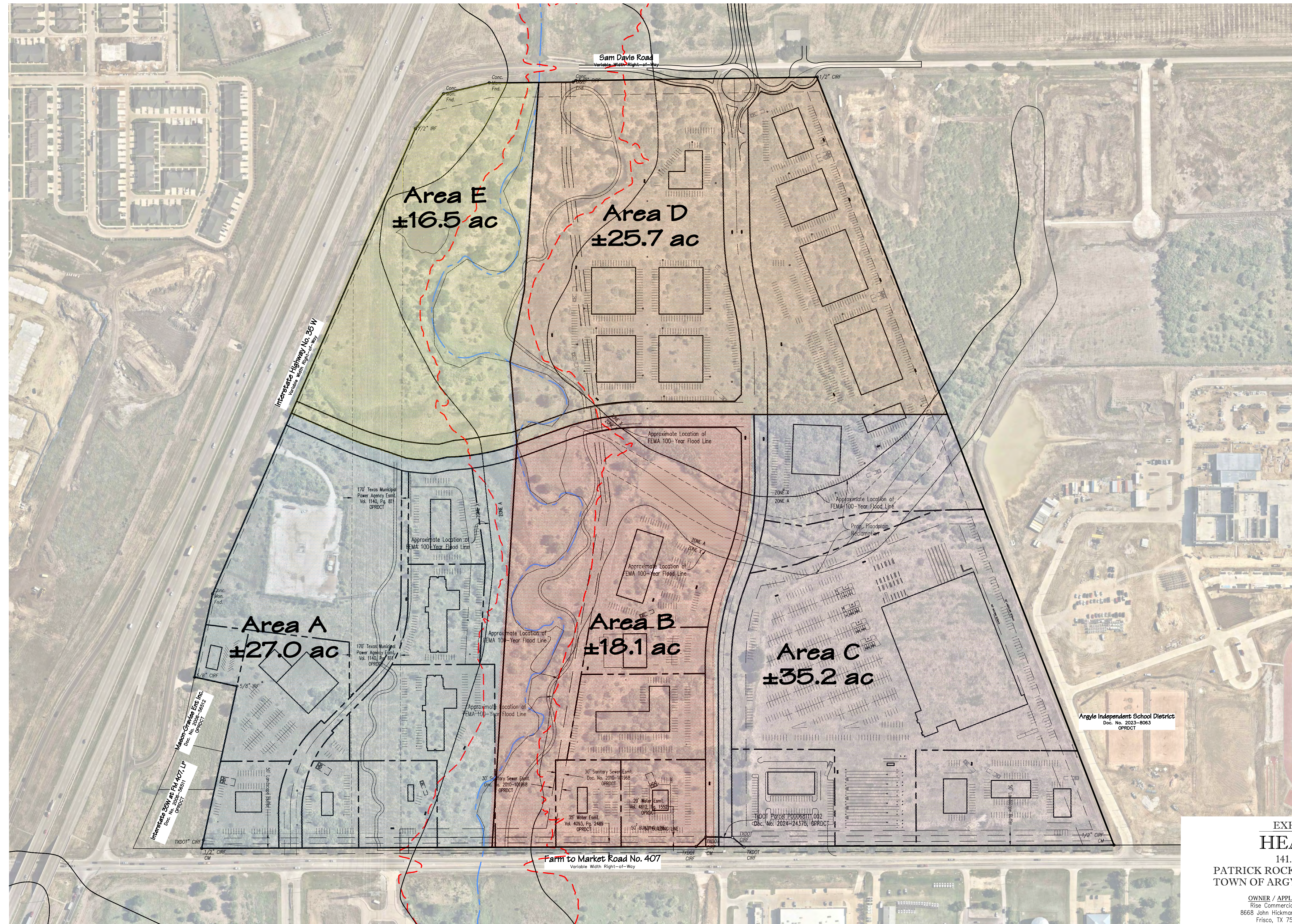
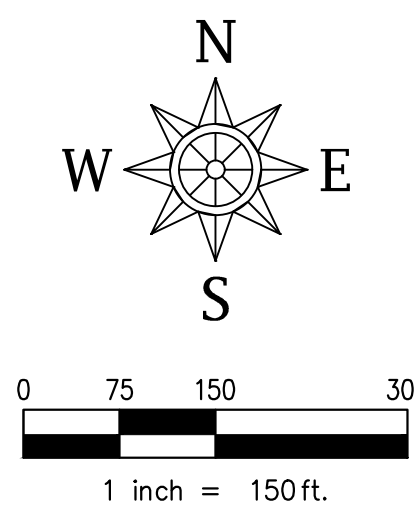
WAREHOUSE CLUB:

A Warehouse Club use is a membership-based warehouse retail use that sells a limited variety of general merchandise and food in bulk quantities to the general public. Products may include foodstuffs, apparel, furniture, appliances, home furnishings, toiletries, cosmetics, photographic equipment, electronics, toys, and other general merchandise. This use requires customers to pay a membership fee to access the exclusive, discounted, bulk-packaged goods. This use is exempt from the Development, Large Scale use definition and requirement of a specific use permit if the building is larger than 40,000 square feet. There is no minimum or maximum square footage for this use.

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### EXHIBIT C: Concept Plan HEATH TRACT

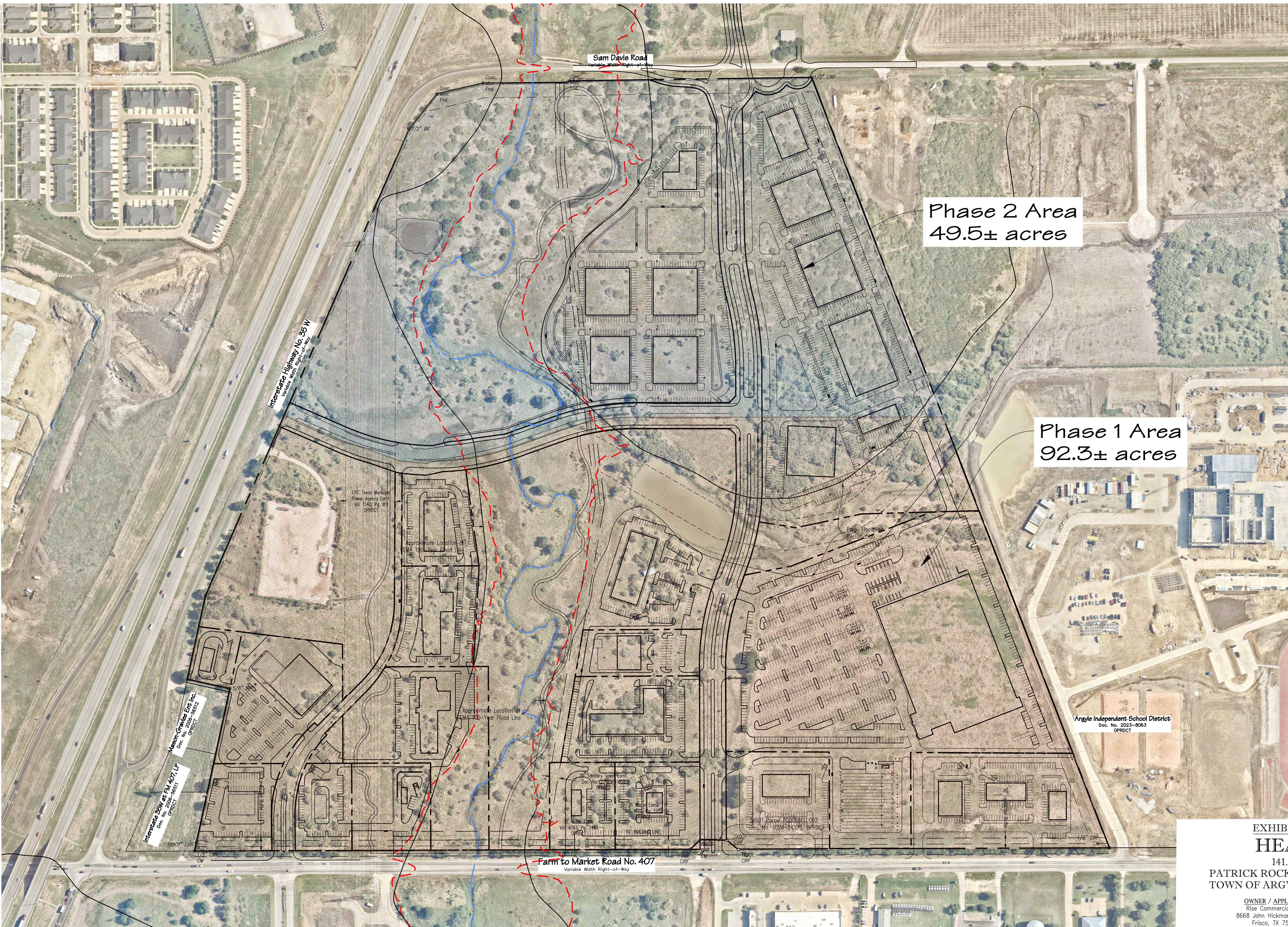
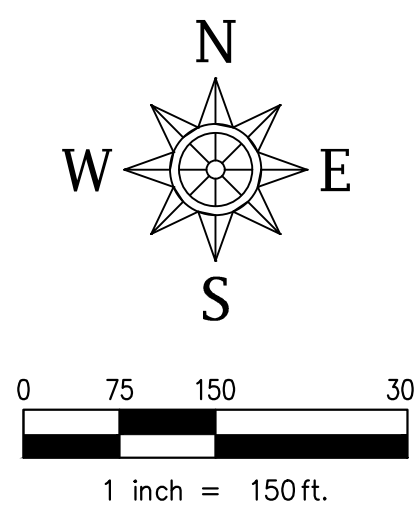
141.672 AC / 6,171,248 SF  
PATRICK ROCK SURVEY ~ ABSTRACT NO. 1063  
TOWN OF ARGYLE, DENTON COUNTY, TEXAS

<b>OWNER / APPLICANT</b> Rise Commercial, LLC 8668 John Hickman Parkway Frisco, TX 75034 Telephone (248) 345-3818 Contact: Shiva Kondru	<b>ENGINEER / SURVEYOR</b> Sparks Engineering, Inc. 3575 Lone Star Circle, Suite 434 Fort Worth, TX 76177 Telephone: (972) 422-0077 TBPELS No. F-2121 and No. F-10043100 Contact: Tyler Barnett
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Scale: 1" = 150' February, 2026 S&E Job No. 25-028

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Phase 2 Area  
49.5± acres

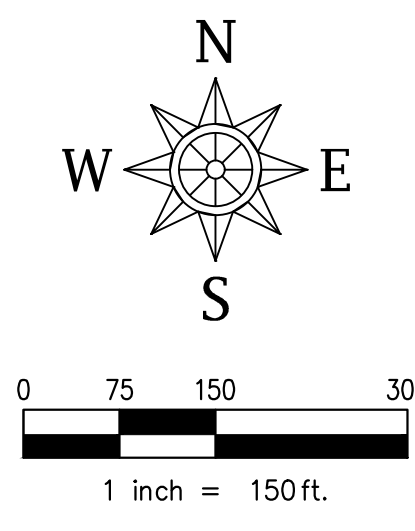
Phase 1 Area  
92.3± acres

**EXHIBIT E: Phasing Plan Exhibit**  
**HEATH TRACT**  
 141.672 AC / 6,171,248 SF  
 PATRICK ROCK SURVEY ~ ABSTRACT NO. 1063  
 TOWN OF ARGYLE, DENTON COUNTY, TEXAS

<b>OWNER / APPLICANT</b> Rise Commercial, LLC 8668 John Hickman Parkway Frisco, TX 75034 Telephone (248) 345-3818 Contact: Shiva Kondru	<b>ENGINEER / SURVEYOR</b> Sparks Engineering, Inc. 3575 Lone Star Circle, Suite 434 Fort Worth, TX 76177 Telephone: (972) 422-0077 TBPELS No. F-2121 and No. F-10043100 Contact: Tyler Barnett
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Scale: 1" = 150' February, 2026 SEI Job No. 25-028

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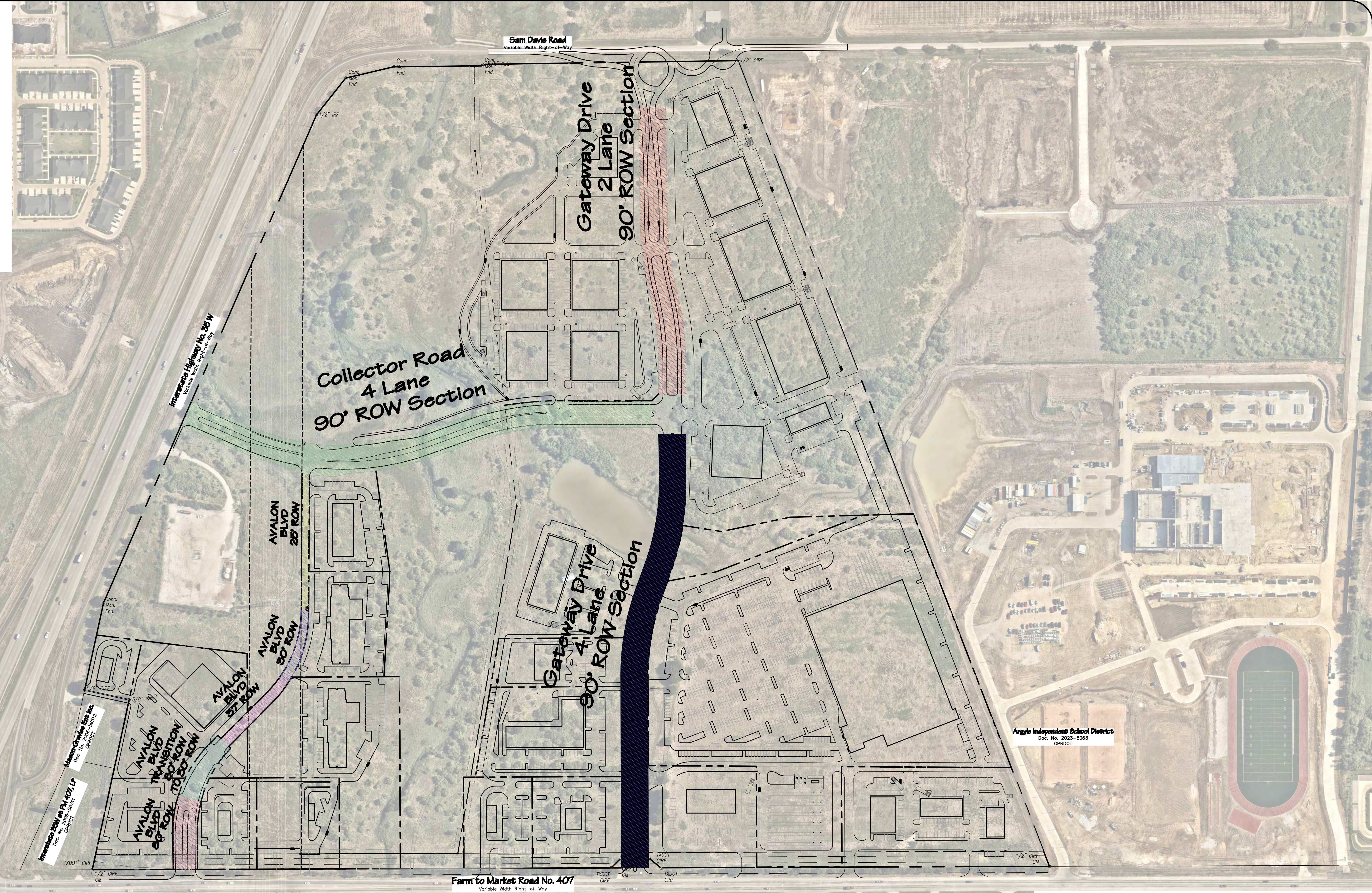
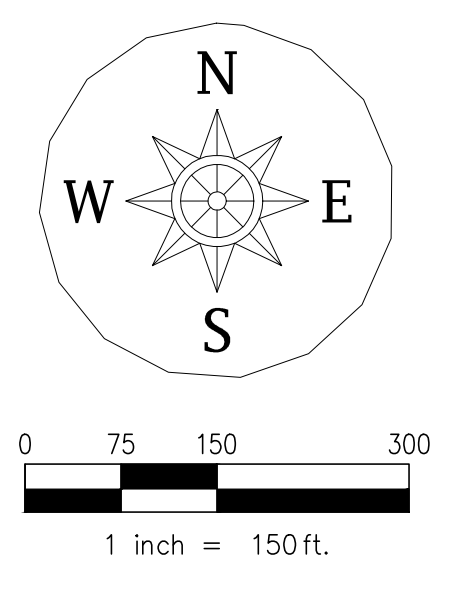


**EXHIBIT F: Open Space Exhibit**  
**HEATH TRACT**  
 141.672 AC / 6,171,248 SF  
 PATRICK ROCK SURVEY ~ ABSTRACT NO. 1063  
 TOWN OF ARGYLE, DENTON COUNTY, TEXAS

<b>OWNER / APPLICANT</b> Rise Commercial, LLC 8668 John Hickman Parkway Frisco, TX 75034 Telephone (248) 345-3818 Contact: Shiva Kondru	<b>ENGINEER / SURVEYOR</b> Sparks Engineering, Inc. 3575 Lone Star Circle, Suite 434 Fort Worth, TX 76177 Telephone: (972) 422-0077 TBPELS No. F-2121 and No. F-10043100 Contact: Tyler Barnett
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This Open Space Exhibit plan is provided for preliminary planning purposes only and is not an official or submitted document. All aerial imagery and mapping are based on the best available information at the time of preparation. The plan is subject to revision.

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 Plotted by: bhinevich1 Plot Date: 2/20/2026 11:46: AM



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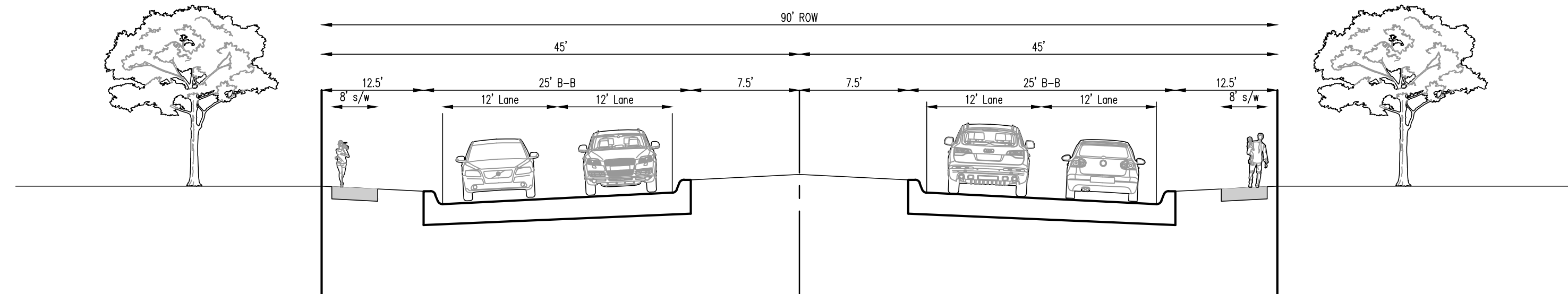
**SPIARS**  
 ENGINEERING & SURVEYING  
 765 Custer Road, Suite 100 • Plano, TX 75075 • 972.422.0077  
 TBPE No F-2121 • TBLPS No. F-10043100 • www.spiarseng.com

This Roadway Sections Exhibit plan is provided for preliminary planning purposes only and is not an official or submitted document. All aerial imagery and mapping are based on the best available information at the time of preparation. The plan is subject to revision.

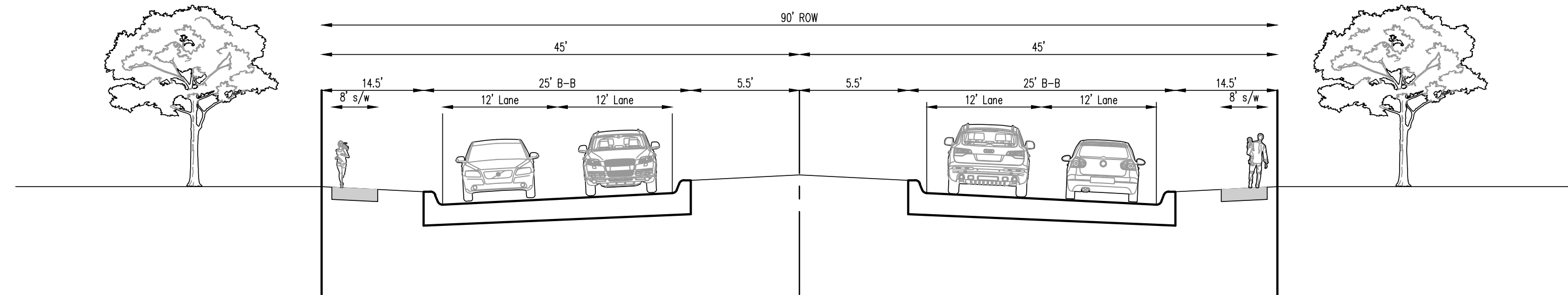
**EXHIBIT G: Roadway Sections Key Map**  
**HEATH TRACT**  
 141.672 AC / 6,171,248 SF  
 PATRICK ROCK SURVEY ~ ABSTRACT NO. 1063  
 TOWN OF ARGYLE, DENTON COUNTY, TEXAS

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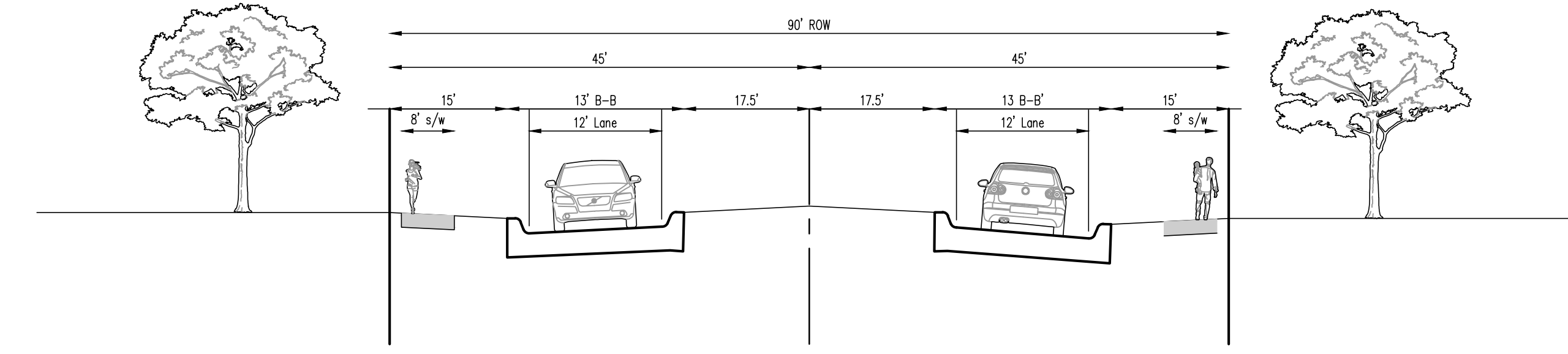
Scale: NTS February, 2026 SEI Job No. 25-028



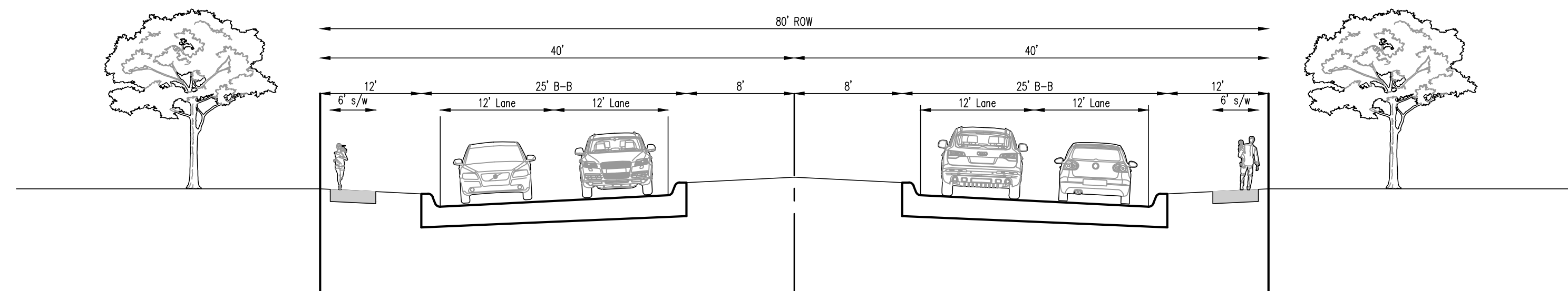
**TYPICAL 90' ROW COLLECTOR ROAD E-W (GREEN)**  
**6' BB PAVEMENT SECTION**  
 NOT TO SCALE



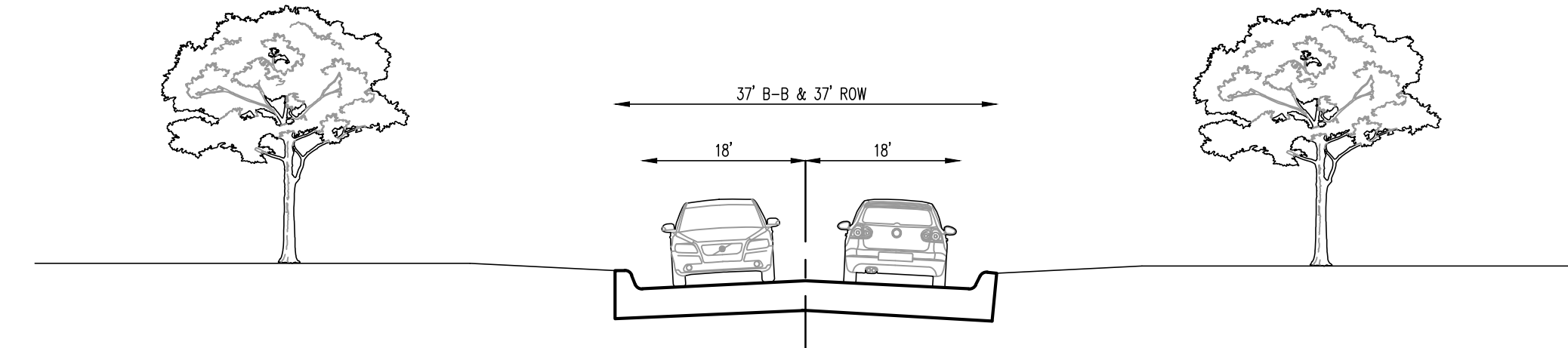
**TYPICAL 90' ROW GATEWAY DRIVE N-S**  
**(SOUTH OF COLLECTOR ROAD)**  
**(BLUE)**  
**6' BB PAVEMENT SECTION**  
 NOT TO SCALE



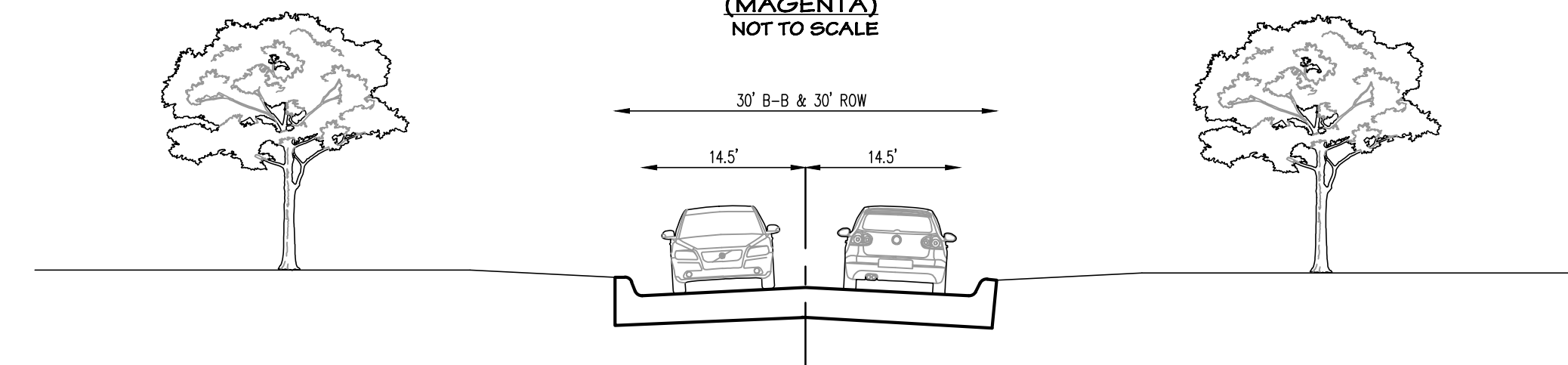
**TYPICAL 90' ROW GATEWAY DRIVE N-S**  
**(NORTH OF COLLECTOR ROAD)**  
**(RED)**  
**6' BB PAVEMENT SECTION**  
 NOT TO SCALE



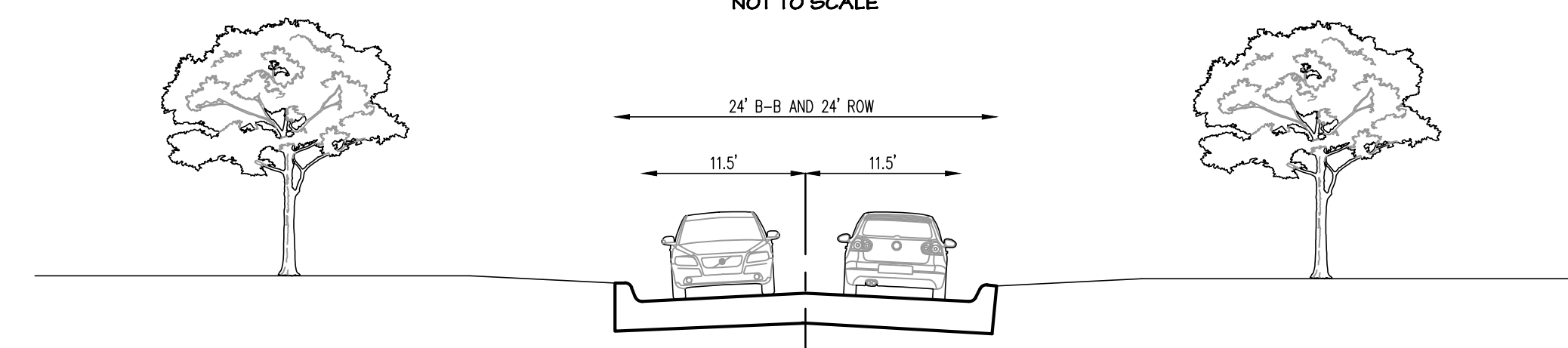
**TYPICAL 80' ROW AVALON ROAD N-E (PINK) 66'**  
**BB PAVEMENT SECTION**  
 NOT TO SCALE



**TYPICAL 37' ROW**  
**(AVALON BLVD NORTH**  
**OF MEDIAN TO EAST**  
**LINE OF TRANSMISSION**  
**EGMT)**  
**(MAGENTA)**  
 NOT TO SCALE



**TYPICAL 30' ROW**  
**(AVALON BLVD EAST**  
**LINE OF TRANSMISSION**  
**EGMT TO COLLECTOR**  
**ROAD**  
**(PURPLE)**  
 NOT TO SCALE



**TYPICAL 24' ROW**  
**(AVALON BLVD EAST**  
**LINE OF TRANSMISSION**  
**EGMT TO COLLECTOR**  
**ROAD**  
**(YELLOW)**  
 NOT TO SCALE

Drawing: C:\2025\_080524-028\_NEC\_S&A\07\CAD\Sheet Sections - P04.dwg, Saved By: shreerajg, Date: 2/20/2026, 11:36:16 AM  
 Plotted by: shreerajg, Plot Date: 2/20/2026, 11:49 AM

**EXHIBIT G: Roadway Sections**  
**HEATH TRACT**  
 141.672 AC / 6,171,248 SF  
 PATRICK ROCK SURVEY ~ ABSTRACT NO. 1063  
 TOWN OF ARGYLE, DENTON COUNTY, TEXAS

**OWNER / APPLICANT**  
 Rise Commercial, LLC  
 8668 John Hickman Parkway  
 Frisco, TX 75034  
 Telephone (248) 345-3818  
 Contact: Shiva Kondra

**ENGINEER / SURVEYOR**  
 Spira Engineering, Inc.  
 3575 Lone Star Circle, Suite 434  
 Fort Worth, TX 76177  
 Telephone: (972) 422-0077  
 TBPELS No. F-2121 and No. F-10043100  
 Contact: Tyler Barnett

This Roadway Sections Exhibit plan is provided for preliminary planning purposes only and is not an official or submitted document. All aerial imagery and mapping are based on the best available information at the time of preparation. The plan is subject to revision.









DECEMBER 19, 2025

# TRAFFIC IMPACT ANALYSIS

To provide Professional Traffic Engineering Services To:

## Rise Commercial

FOR

### Heath Tract Infrastructure in Argyle, Texas

For submittal to the Town of Argyle



Presented by:



765 Custer Road, Suite 100 | Plano, Texas 75075  
Main 972.422.0077 | [www.spiarseng.com](http://www.spiarseng.com)  
TBPE Reg. No. F-2121 | TBPLS Reg. No. F-100

## EXECUTIVE SUMMARY

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Spiars Engineering & Surveying was retained by Rise Commercial to provide a Traffic Impact Analysis (TIA) for the proposed Commercial development. The proposed development is to be located on the north side of the intersection of FM 407 and Gateway Drive in Argyle, Texas. For the purposes of this report, the Development is referred to herein as “Heath Tract Infrastructure”. The Development is proposed to consist of restaurant, hotel, and other retail uses and is expected to be ready for occupancy by 2035. The development proposes to have access along the future I-35W Northbound Frontage Road, FM 407 and the future extension of Gateway Drive.

Based on the Town of Argyle’s Development Standards Section 14.2.90.C.6 stating that the minimum acceptable congestion level of Level of Service (LOS) C, the following principal findings were found as part of Spiars’ analyses of projected operational conditions at the “Build” condition.

### Project Background Findings

- Roadway Improvements to I-35W and FM 407 within the project buildout were made known to Spiars at the onset of the traffic study. I-35W will be reconfigured from a 4-lane rural interstate to a six-lane urban freeway with an off-ramp connection to the northbound frontage road just north of FM 407. The interchange with FM 407 will also include a u-turn lane for the southbound frontage road in connection with the northbound frontage road. Additionally, FM 407 will be reconstructed from a 2-lane highway to a four-lane median-divided highway from I-35W to Gateway Drive. These assumptions have been applied to the analysis in the year 2030 and 2035 scenarios.
- In addition to the above improvements, the Town will install traffic signals at the intersections of FM 407 at Avalon Boulevard and FM 407 at Gateway Drive. For purposes of this study, traffic signals have been assumed at these intersections for the No-Build and Build Conditions.

### Level of Service Findings

- All intersections and roadways (FM 407) within the study area operate at “acceptable” overall intersection Levels of Services during the Existing Conditions. With the addition of estimated background traffic growth and estimated site development traffic, the overall intersection Levels of Service are projected to continue to operate at “acceptable” Levels of Service with slight increases to the delays except for the following intersections.
- The intersection of I-35W and the Southbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below.

(Continued)...

- The intersection of I-35W and the Northbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below. Mitigations for the degrading conditions are listed in the table below.
- The exiting southbound maneuvers at Drive 2 along FM 407 are projected to operate at “unacceptable” Levels of Service in the project Build Conditions in both the AM and PM Peak Hours. These “unacceptable” conditions are typical of other similar unsignalized intersections and driveways along major high-volume roadways such as FM 407 and can only be mitigated with the installation of a traffic signal. However, with the anticipated low traffic volumes exiting the minor approach at the driveway from the development, the warrants for a traffic signal are estimated to not be met. Therefore, no mitigations for the “unacceptable” Levels of Service are recommended.

Site Access Findings

- According to the TxDOT guidelines for the installation of a left-turn deceleration lane, it is recommended that the development install eastbound left-turn deceleration lanes at all driveways with full left-turn access along FM 407. Driveways required to have the installation of a left-turn lane include Avalon Boulevard, Gateway Drive, and Drive 2.
- According to the TxDOT guidelines for the installation of a right-turn deceleration lane, it is recommended that the development install a westbound right-turn deceleration lane at Avalon Boulevard for traffic entering the development.

(Continued)...

Traffic Impact Analysis Mitigations Table

Scenario	Year	Intersection/Approach/Peak	Recommendation	Responsible Party	Pro-Rata
Build	2035	FM 407 and I-35W Southbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2035	FM 407 and I-35W Northbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2028	FM 407 and Avalon Boulevard	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Drive 2	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Westbound Right-Turn Lane	Development	100%

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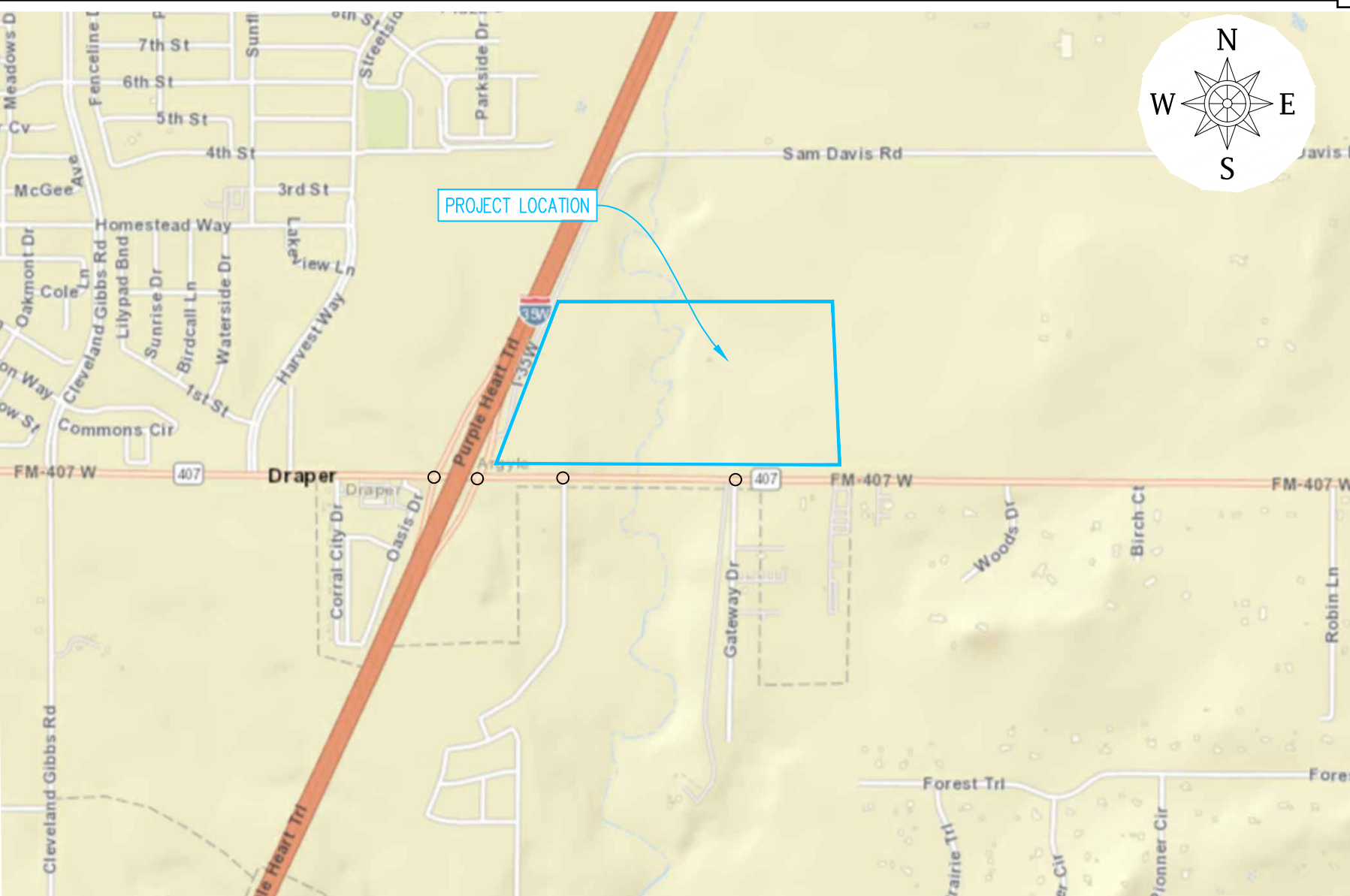
- Table 1. Annual Calculated Growth Rate Summary*
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LIST OF EXHIBITS:

- Exhibit 1. Site Location Map*
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LIST OF APPENDICES:

- Appendix A – Existing Data Collection and Information*
- Appendix B – Trip Generation and Traffic Assignment*
- Appendix C – Traffic Volume Exhibits*
- Appendix D – Synchro Analysis*



LEGEND:

- PROJECT LOCATION
- STUDY INTERSECTION



**EXHIBIT 1 - VICINITY MAP**  
**HEATH TRACT INFRASTRUCTURE**  
**CITY OF ARGYLE**  
**DENTON COUNTY, TEXAS**

Drawn:	Checked:	Date	Job No.
HWL	HWL	6/19/25	25-028

Plotted by: henry Plot Date: 6/19/2025 9:14 AM



## INTRODUCTION AND BACKGROUND

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### *Purpose*

Spiars Engineering & Surveying was retained by Rise Commercial to provide a Traffic Impact Analysis (TIA) for the proposed Commercial development. The proposed development is to be located on the north side of the intersection of FM 407 and Gateway Drive in Argyle, Texas. A vicinity map is provided in **Exhibit 1**.

The purpose of the study is to estimate the traffic impacts of the proposed development in reference to the current traffic conditions of the immediate surrounding vicinity of the property. These impacts are estimated by projecting the development trips arriving and departing the site and developing the traffic patterns to the immediate surrounding intersections to the property. Once the impacts are identified, the findings of the study are to determine the necessary mitigations, if any, needed to maintain acceptable traffic conditions based on SPIARS professional opinions and/or recommendations. Necessary mitigations may include, but are not limited to, safety, overall traffic operations, site access, circulation, etc. Execution and completion of the necessary mitigations are subject to approval of the agency that is responsible for the operation of the transportation facilities. NOTE: The mitigations presented in the analysis of this study should not be regarded as required and not intended to assign financial responsibility to any parties.

A TIA is required by the Town of Argyle as part of the Town's site plan permitting process outlined in the Town of Argyle Development Standards Section 14.2.90.C. This report was prepared by professional expertise in accordance with industry standards by SPIARS traffic engineers that follows the Town's applicable TIA Guidelines and guidance provided by the Town's staff. In cases where parameters were not discussed, SPIARS used professional judgement, as applicable, in order to finalize this report.

### *Project Description*

The development proposes to consist of multiple retail uses throughout the site and is expected to be ready for occupancy by 2035. A site plan is provided in **Exhibit 2**.

Additionally, the development is proposing to construct three new driveways along FM 407 with access along the future extension of Gateway Drive, as well. For purposes of this study, FM 407 will be improved from the current two-lane cross-section to a four-lane, median divided roadway by the completion of the subject development. It is assumed that the improvements will end at Gateway Drive, and therefore, all access points east of Gateway Drive will have full right- and left-turn access. All information pertaining to existing conditions provided within this report is supported and validated by on-site observations conducted at the outset of the study.

Roadway Improvements to I-35W and FM 407 within the project buildout were made known to Spiars at the onset of the traffic study. I-35W will be reconfigured from a 4-lane rural interstate to a six-lane urban freeway with an off-ramp connection to the northbound frontage road just north of FM 407. The interchange with FM 407 will also include a u-turn lane for the southbound frontage road

in connection with the northbound frontage road. Additionally, FM 407 will be reconstructed from a 2-lane highway to a four-lane median-divided highway from I-35W to Gateway Drive. These assumptions have been applied to the analysis in the year 2030 and 2035 scenarios.

### Study Parameters

In accordance with the Town’s TIA guidelines and staff direction, this TIA includes analyses of traffic operations for the following scenarios listed below. All parameters mentioned in this study were reviewed and approved by the Approving Agency at the commencement of the TIA.

#### Scenario Parameters

- “Existing” Conditions (2025)
- 2030 “Build” Conditions
- 2030 “No-Build” Conditions
- 2035 “Build” Conditions

#### Analysis Area Parameters

Per Town direction, the specific analyses performed in this study include Intersection Capacity, including AM and PM peak hour Level of Service by movement, delay by movement, and queuing analysis with 95<sup>th</sup> percentile queue lengths by movement for the following study area intersections:

- Intersections
  - I-35W SBFR and FM 407
  - I-35W NBFR and FM 407
  - FM 407 and Avalon Boulevard
  - FM 407 and Gateway Drive
  - Site Driveways

And Roadway Link Analysis to determine the appropriate capacity for the type of roadway for the following roadway segments:

- Roadway Links
  - FM 407
    - Existing Cross-Section: Two-lane, undivided roadway
    - Town of Argyle Thoroughfare Plan Designation: Type A Major Arterial (6 lanes, median-divided, 120’ R.O.W.)
    - Existing Daily Traffic Count: 14,259 (SPIARS - 2025)
    - Posted Speed Limit: 55 mph

- Gateway Drive
  - Existing Cross-Section: Six-lane, median-divided roadway
  - Town of Argyle Thoroughfare Plan Designation: Proposed Type B Minor Arterial (4 lanes, median-divided, 90' R.O.W.)
  - Existing Daily Traffic Count: Not Available
  - Posted Speed Limit: Not Available

## TRAFFIC IMPACT ANALYSIS

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### *Existing Traffic Data Collection and Analyses*

Current traffic volumes commissioned by SPIARS were collected on Thursday, April 24, 2025, by a local traffic counting subcontractor, CJ Hensch & Associates. The peak hours that were selected for this study consist of each individual intersection peak hour, as a conservative approach. Raw detailed datasheets from CJ Hensch & Associates compiled into SPIARS formatting datasheets are provided in **Appendix A**.

NOTE: To ensure that automated traffic data captured demand, SPIARS researched multiple TxDOT historical hourly counts at multiple locations. In this research, SPIARS found the initial two-hour window for both AM and PM peak periods. From there, the data presented a peak hour window to be selected for the entire study area. Through observations from field and video, Spiars was able to validate the data to be accurate of current conditions. No short-term developments are proposed at this site other than the proposed development. Any developments to be completed by the subject development's buildout year are provided in the analysis as background traffic in the "No-Build" Conditions analyses.

### *"No-Build" Traffic Volume Data and Analyses*

The "No-Build" scenario incorporates anticipated changes to the "Existing" conditions that are unrelated to and independent of the subject development but that are expected to be in place at the time of development's time of completion. Changes may include increases in traffic volumes, either through background growth or new developments in the area, and/or improvements to the existing roadway network. Those changes are presented below.

According to the Town's Thoroughfare Plan, the following transportation infrastructure improvements are planned and are anticipated to be in place by the time of the completion of the subject development. The Town's documentation provides a graphical depiction of the roadway network conditions anticipated to exist for this scenario. These improvements are included in the analyses at the "No-Build" Conditions analysis scenario.

- FM 407
  - Cross-Section: 6 lanes with median from I-35W to Gateway Drive

- Gateway Drive
  - Cross-Section: 4 lanes with median north of FM 407

In order to account for anticipated growth of background traffic within the study area, historical traffic volumes collected by the Texas Department of Transportation (TxDOT) were obtained from the TxDOT Traffic Count Database System (TCDS) and provided in a screenshot in **Appendix A**. The growth rate for the station presented below is calculated separately using the compound growth formula. The data used in the calculation in the table below was found at the nearest applicable location with, to the furthest extent possible, enough yearly data (3+ year separation where available) to calculate a projection. The data used in this analysis are summarized in **Table 1**.

*Table 1. Annual Calculated Growth Rate Summary*

LOCATION	VOLUME	PERCENTAGE
FM 407, west of FM 973	19,925 ('22)	2.33% ~ 2.50%
	16,868 ('18)	

From the calculations presented above, a growth percentage of 2.5% was used in this study. Study traffic volumes with the projected growth rate(s) applied are graphically summarized in **Appendix C**.

### *Development Related Traffic Volumes and Analyses*

To determine the traffic volumes generated by the proposed development, SPIARS applied the standard process of estimating trip generation, mode split, trip distribution, and traffic assignment. Trip generation for the subject development within this study was calculated using the methodologies presented by the Institute of Transportation Engineers (ITE) *Trip Generation* handbook, 12<sup>th</sup> Edition. The projected daily and peak hour trip generation for the proposed development is summarized in **Table 2** (see also **Appendix B**).

Table 2. Development Calculated Trip Generation Summary

YEAR	AREA	ITE CODE #	LAND USE	UNITS	(WEEKDAY)	AM PEAK HOUR			PM PEAK HOUR		
						Total	In	Out	Total	In	Out
2030	A	310	Hotel	240 Rooms	1,402	82	42	40	113	58	55
		822	Strip Retail Plaza	39,000 SF	2,124	153	84	69	245	123	122
	D	820	Shopping Center	246,000 SF	8,952	216	134	82	802	393	409
2035	B	821	Shopping Plaza	113,000 SF	7,388	180	111	69	538	264	274
	C	820	Shopping Center	170,000 SF	6,186	150	93	57	554	272	282
Pass-By Trips <sup>1</sup>					<u>0</u>	<u>178</u>	<u>110</u>	<u>68</u>	<u>608</u>	<u>298</u>	<u>310</u>
TOTAL:					26,052	603	354	249	1,644	812	832

<sup>1</sup>More information provided in Appendix B

The trip generation calculations provided by ITE typically are calculated by either a range of rates or by fitted curve equations. In this case, the average rates were chosen as worst case and are represented in a Land Use Detail spreadsheet provided in **Appendix B**.

At the discretion of the Engineer, adjustments to the standard trip generation may apply in order to account for development-specific factors. For this analysis, reductions to the calculated trip generation by assumptions made for Internal Trip Capture were not considered significant enough to be applied within the analysis as these metrics are already determined within the Land Use Code used for each portion of the development. However, reductions to the calculated trip generation by assumptions made for Pass-by Trips were considered significant enough to be applied within the analysis.

Pass-by Trips are an estimated determination of the amount of “background” traffic volumes whose vehicle travel path passes by the future proposed development. In this case, the estimation that is used for pass-by reduction refers to the percentage of the development’s site-related traffic that is already accounted for within the existing traffic collection data. Furthermore, the existing traffic collection data assumed to access this site is projected to only alter the subject vehicles travel path simply only to access the site’s driveway along a particular roadway.

Through the ITE Trip Generation Manual, 11<sup>th</sup> Edition, Pass-by trip percentages for Land Use Code 820 are 29% in the PM Peak Hour and for Land Use Code 821 are 40% in the PM Peak Hour. Excerpts from ITE are provided in **Appendix B**.

Mode Split is an estimated determination of the site-related traffic that is projected to utilize other modes of transportation other than accessing the site as a vehicular trip. Modes of transportation that enter into this category are such as arriving and departing the subject site via walking, bus ridership, carpooling, passenger train, etc. For this analysis, reductions to the calculated trip

generation by assumptions made for Mode Split were not considered significant enough to be applied within the analysis as there are no significant transit routes nearby.

To estimate trip distribution, SPIARS first performed a “cordon line” analysis using existing peak hour volumes approaching the site during the peak hours to obtain the relative percentage of traffic on each corridor. Then, based on professional engineering judgement, resulting percentages were estimated through orientation of travel through general vehicular traffic patterns and characteristics by local roadway networks. These resulting percentages are summarized in Appendix B. Communications with the developer about anticipated traffic patterns were also in place for the finalization of the trip distribution.

Once the site-generated traffic was distributed to the major travel directions, the resulting traffic percentages were assigned to specific traffic routes and turning maneuvers through immediate surrounding intersections. These assignments were based upon the location of proposed site access and the assumed best and most-direct travel path to/from the nearest and most appropriate site driveway(s). A graphical summary of projected site-related traffic volumes is provided in **Appendix C**.

### **“Build” Traffic Volume Data and Analyses**

Traffic volumes for the “Build” condition are a sum of the “No-Build” traffic volumes, plus the projected development-related traffic volumes. The resulting calculated “Build” traffic volumes are summarized in **Appendix C**.

When unacceptable conditions arise as presented in the analyses of this study, mitigations measures may be required by the Approving Agency to restore conditions back to acceptable conditions.

### **Intersection Capacity Analysis**

As required by the Approving Agency, SPIARS conducted peak hour traffic intersection capacity analysis of the study area intersections and applicable development driveways for the “Existing”, “No Build”, and “Build” scenarios. For peak hour intersection operations, analyses were performed using the latest version of the *Synchro* analysis software to apply methodologies from the latest Highway Capacity Manual (HCM – 7<sup>th</sup> Edition).

The HCM considers the quantitative stratification of a performance measure(s) representing the quality of a transportation system is referred to as the Level of Service (LOS) of a particular facility. The LOS concept facilitates the presentation of results using a familiar A (best) to F (worst) letter grade scale. Control delay is the principal HCM service measure for evaluating LOS at signalized and unsignalized intersections. Control delay consists of vehicles slowing in advance of an intersection, time spent stopped on an intersection approach, time spent as vehicles move up in the queue, and time needed for vehicles to accelerate to their desired speed.

Generally, the HCM LOS can be described as progression is exceptionally favorable (LOS A), progression is highly favorable (LOS B), progression is favorable or moderate (LOS C), progression is ineffective (LOS D), progression is unfavorable (LOS E), and progression is very poor (LOS F). Traffic operational analysis is typically measured in one-hour periods during day-to-day peak conditions. On

high volume roadways and intersections, the peak Levels of Service that are unfavorable are not uncommon. Based on the Town of Argyle’s Development Standards Section 14.2.90.C.6, the minimum acceptable congestion level is LOS C.

The following table summarizes the LOS criteria for signalized and unsignalized intersections as defined in the latest edition of the Highway Capacity Manual.

LOS	Signalized Intersection Control Delay (s/veh)	Unsignalized Intersection Control Delay (s/veh)
A	≤ 10	≤ 10
B	> 10 - ≤ 20	> 10 - ≤ 15
C	> 20 - ≤ 35	> 15 - ≤ 25
D	> 35 - ≤ 55	> 25 - ≤ 35
E	> 55 - ≤ 80	> 35 - ≤ 50
F	> 80	> 50

As observed from field and video observations of current conditions, operational results retrieved from software analyses are representative and validated to be accurate of what was observed in the field and video.

In order to illustrate conditions as realistic as possible, the following parameters were applied in the analyses:

- For all future traffic signals, traffic signal timing was inputted by professional traffic engineering judgement
- With the new construction of the intersection of FM 407 and Gateway Drive, a peak hour factor of 0.92 for all movements was used for all scenarios.

Results for the peak hour Level of Service, delay, and 95th percentile queue were summarized by movement and by overall intersection for each analysis scenario and are presented in **Table 3**.

Due to limitations of the HCM 7<sup>th</sup> Edition methodology results used in this analysis (HCM 7<sup>th</sup> Edition methodology does not produce Synchro Report results due to clustered intersections not being supported, the interchange of I-35W and FM 407 was required to use the Synchro generated methodology (i.e. the “Percentile Delay Method” -- Synchro defines the Percentile Delay Method as calculations that are interval based. “Vehicle arrivals from adjacent intersections are evaluated in intervals to determine the influence of coordination”).



Table 3. Intersection Peak Hour Operational Results

INTERSECTION	Approach	EXISTING CONDITIONS						2030 NO BUILD CONDITIONS						2030 BUILD CONDITIONS						2035 BUILD CONDITIONS					
		AM Peak Hour of Adj Street			PM Peak Hour of Adj Street			AM Peak Hour of Adj Street			PM Peak Hour of Adj Street			AM Peak Hour of Adj Street			PM Peak Hour of Adj Street			AM Peak Hour of Adj Street			PM Peak Hour of Adj Street		
		V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS	V/C Ratio	delay (s)	LOS
I-35W SBFR @ FM 407	Overall	--	(18.2)	B	--	(19.3)	B	--	(18.2)	B	--	(18.8)	B	--	(19.3)	B	--	(19.9)	B	--	(22.2)	C	--	(27.6)	C
	EBT	0.74	(32.5)	C	0.79	(35.8)	D	0.72	(21.2)	C	0.81	(24.2)	C	0.73	(21.7)	C	0.83	(25.8)	C	0.83	(26.3)	C	0.96	(38.8)	D
	EBR	0.45	(4.4)	A	0.52	(4.7)	A	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	--	
	WBL	0.66	(31.5)	C	0.52	(27.9)	C	0.72	(38.0)	D	0.58	(32.8)	C	0.79	(40.3)	D	0.75	(34.9)	C	0.86	(44.3)	D	0.91	(40.9)	D
	WBT	0.44	(12.4)	B	0.65	(20.6)	C	0.24	(7.1)	A	0.37	(10.8)	B	0.24	(5.9)	A	0.34	(6.8)	A	0.26	(5.7)	A	0.37	(6.1)	A
	SBL	0.17	(22.2)	C	0.13	(19.9)	B	0.12	(24.8)	C	0.09	(22.0)	C	0.19	(26.4)	C	0.11	(25.6)	C	0.25	(27.9)	C	0.14	(26.6)	C
	SBT	0.39	(5.5)	A	0.40	(5.0)	A	0.07	(23.7)	C	0.06	(21.2)	C	0.11	(24.7)	C	0.08	(24.7)	C	0.14	(25.7)	C	0.09	(25.5)	C
	SBR	--	--	--	--	--	--	0.45	(5.6)	A	0.48	(9.3)	A	0.46	(5.9)	A	0.55	(12.2)	B	0.54	(7.1)	A	0.72	(23.1)	C
I-35W NBFR @ FM 407	Overall	--	(16.1)	B	--	(19.5)	B	--	(15.4)	C	--	(16.4)	B	--	(17.0)	B	--	(21.0)	C	--	(20.2)	C	--	(34.6)	C
	EBL	0.49	(11.8)	B	0.60	(13.2)	B	0.46	(13.2)	B	0.60	(16.1)	B	0.46	(17.1)	B	0.61	(24.7)	C	0.53	(21.8)	C	0.65	(26.4)	C
	EBT	0.38	(6.0)	A	0.35	(4.7)	A	0.21	(5.6)	A	0.20	(5.4)	A	0.23	(6.6)	A	0.24	(4.7)	A	0.26	(6.7)	A	0.28	(4.7)	A
	WBTR	0.59	(27.6)	C	0.60	(39.7)	D	0.43	(21.3)	C	0.46	(21.3)	C	0.50	(22.3)	C	0.72	(25.8)	C	0.60	(24.1)	C	0.94	(43.3)	D
	NBLTR	0.61	(32.1)	C	0.76	(38.5)	D	0.56	(18.3)	B	0.56	(18.8)	B	0.64	(19.8)	B	0.76	(24.8)	C	0.78	(25.6)	C	0.97	(46.6)	D
FM 407 @ Avalon Boulevard	Overall	--	(2.0)	A	--	(1.3)	A	--	(13.1)	B	--	(13.1)	B	--	(12.3)	B	--	(13.4)	B	--	(12.7)	B	--	(14.4)	B
	EBL	--	--	--	--	--	--	--	--	--	--	--	0.21	(11.8)	B	0.39	(16.4)	B	0.23	(12.0)	B	0.50	(22.8)	C	
	EBT	--	--	--	--	--	0.60	(15.0)	B	0.67	(14.5)	B	0.62	(14.6)	B	0.73	(15.3)	B	0.68	(15.0)	B	0.76	(15.7)	B	
	WBL	0.025	(8.8)	A	0.044	(9.6)	A	0.15	(11.9)	B	0.24	(13.6)	B	0.16	(11.8)	B	0.27	(14.4)	B	0.19	(12.2)	B	0.37	(19.0)	B
	WBT	--	--	--	--	--	0.41	(12.8)	B	0.43	(11.9)	B	0.45	(12.2)	B	0.57	(12.6)	B	0.50	(12.2)	B	0.64	(13.2)	B	
	NBL	0.236	(24.7)	C	0.179	(32.8)	D	0.09	(8.0)	A	0.05	(8.5)	A	0.10	(8.5)	A	0.06	(10.5)	B	0.12	(9.1)	A	0.08	(14.4)	B
	NBTR	0.097	(12.0)	B	0.060	(13.2)	B	0.04	(0.6)	A	0.02	(0.3)	A	0.04	(1.2)	A	0.03	(1.7)	A	0.05	(3.3)	A	0.03	(3.3)	A
	SBL	--	--	--	--	--	--	--	--	--	--	--	--	0.05	(8.2)	A	0.08	(10.7)	B	0.05	(8.6)	A	0.09	(14.5)	B
	SBTR	--	--	--	--	--	--	--	--	--	--	--	--	0.03	(0.0)	A	0.06	(1.5)	A	0.03	(0.0)	A	0.07	(4.2)	A
	FM 407 @ Gateway Drive	Overall	--	(0.8)	A	--	(3.2)	A	--	(7.6)	A	--	(8.3)	A	--	(7.6)	A	--	(8.4)	A	--	(7.9)	A	--	(10.5)
EBL		--	--	--	--	--	--	--	--	--	--	--	0.07	(6.5)	A	0.25	(8.8)	A	0.17	(7.7)	A	0.62	(19.4)	B	
EBTR		--	--	--	--	--	0.35	(7.6)	A	0.37	(8.1)	A	0.38	(7.9)	A	0.44	(8.8)	A	0.42	(8.3)	A	0.49	(9.3)	A	
WBL		0.032	(9.3)	A	0.030	(9.6)	A	0.08	(6.9)	A	0.08	(6.8)	A	0.09	(7.0)	A	0.09	(7.2)	A	0.12	(7.4)	A	0.13	(7.9)	A
WBTR		--	--	--	--	--	0.20	(6.9)	A	0.23	(7.1)	A	0.23	(7.1)	A	0.31	(7.6)	A	0.27	(7.2)	A	0.38	(7.9)	A	
NBL		--	--	--	--	--	0.06	(24.1)	C	0.19	(26.0)	C	0.06	(24.1)	C	0.21	(26.5)	C	0.07	(24.3)	C	0.30	(28.8)	C	
NBTR		0.107	(18.5)	C	0.493	(33.4)	D	0.01	(0.0)	A	0.07	(0.1)	A	0.01	(0.0)	A	0.07	(0.1)	A	0.02	(0.0)	A	0.08	(1.4)	A
SBTR		--	--	--	--	--	--	--	--	--	--	--	--	0.01	(23.3)	C	0.05	(23.9)	C	0.07	(24.2)	C	0.30	(27.9)	C
FM 407 @ Drive 1	Overall	--	--	--	--	--	--	--	--	--	--	--	--	(0.0)	A	--	(0.1)	A	--	(0.0)	A	--	(0.1)	A	
	SBR	--	--	--	--	--	--	--	--	--	--	--	0.004	(9.0)	A	0.019	(9.3)	A	0.004	(9.1)	A	0.020	(9.5)	A	
FM 407 @ Drive 2	Overall	--	--	--	--	--	--	--	--	--	--	--	--	(0.6)	A	--	(5.9)	A	--	(0.7)	A	--	(24.5)	C	
	EBL	--	--	--	--	--	--	--	--	--	--	--	0.031	(8.6)	A	0.096	(9.2)	A	0.033	(8.9)	A	0.11	(9.9)	A	
	SBL	--	--	--	--	--	--	--	--	--	--	--	0.071	(33.0)	D	0.77	(162.1)	F	0.119	(54.6)	F	2.285	(976.4)	F	
	SBR	--	--	--	--	--	--	--	--	--	--	--	0.034	(11.8)	B	0.191	(14.0)	B	0.038	(12.7)	B	0.234	(16.6)	C	
I-35W NBFR @ Drive 3	Overall	--	--	--	--	--	--	--	--	--	--	--	--	(0.5)	A	--	(0.7)	A	--	(0.5)	A	--	(0.6)	A	
	WBR	--	--	--	--	--	--	--	--	--	--	--	0.030	(9.6)	A	0.052	(10.2)	B	0.032	(9.9)	A	0.06	(10.9)	B	
I-35W NBFR @ Drive 4	Overall	--	--	--	--	--	--	--	--	--	--	--	--	(0.0)	A	--	(0.2)	A	--	(0.4)	A	--	(0.9)	A	
	WBR	--	--	--	--	--	--	--	--	--	--	--	0.007	(9.5)	A	0.013	(10.0)	A	0.026	(10.0)	A	0.101	(11.4)	B	

NOTE: Traffic signal operational parameters used in this analysis were based upon actual, field conditions. LOS = Level of Service

### Roadway Link Capacity Analysis

As required in the approved the Approving Agency TIA Scope, a roadway link capacity analysis was performed for each roadway within the scope. A segment of roadway between intersections is considered to be a roadway link. The general roadway link analysis methodology includes comparing the projected daily traffic volume with the theoretical maximum capacity. The theoretical maximum capacity for roadway links for this study utilizes The North Central Texas Council of Governments (NCTCOG) methodology which outlines the hourly service volume capacities as shown in the table below.

*NCTCOG Roadway Link Hourly Service Volumes By Function*

Area Type	Principal Arterial		Minor Arterial & Frontage Road		Collector & Local Street	
	Median Divided or One-Way	Undivided, Two-Way	Median Divided or One-Way	Undivided, Two-Way	Median Divided or One-Way	Undivided, Two-Way
CBD	725	650	725	650	475	425
Urban/Commercial	850	775	825	750	525	475
Suburban Residential	925	875	900	825	575	525
Rural	1,025	925	975	875	600	550

Level of Service (LOS) for each roadway link is determined by evaluating the volume:capacity (V/C) ratio and comparing to each Level of Service letter grade as shown in the table below. The V/C for a roadway segment is determined by using the calculated traffic volumes for each analysis scenario and dividing by the theoretical capacity, as shown above. Typically, when LOS exceeds “LOS D” (or 80%), the Approving Agency will require that the roadway be recommended for upgrade.

Level of Service (LOS)	V/C Ratio
LOS A / LOS B	≤ 45 %
LOS C	> 45% - ≤ 65%
LOS D	> 65% - ≤ 80%
LOS E	> 80% - ≤ 100%
LOS F	> 100%

By adding the same projected background growth, applicable estimated adjustment factors, and site-generated traffic to the existing daily volumes, the “Existing”, “No Build”, and “Build” volumes were determined for each study area roadway segment. The roadway segment capacity was also determined for each analysis scenario based upon the existing roadway cross-section, plus any assumed improvements, if applicable. The results of the Roadway Link Analysis are summarized in **Table 4.**

*Table 4. Roadway Link Operational Results*

ROADWAY/ SCENARIO	CALCULATED PEAK VOLUME (PM)			FUNCTIONAL CLASS
	EXIST	NO BUILD	BUILD	
<u>FM 407</u>				
Existing Cross- Section	1,014	1,147	2,089	None
Proposed Cross-Section (Capacity)	1,850	3,700	3,700	Type A
V:C RATIO	0.55	0.31	0.56	--
LOS	C	B	C	--

## SUPPLEMENTAL ANALYSIS

### *Driveway Spacing*

As required by the Town, Driveway Spacing for the subject development was analyzed for all new site access driveways that are proposed along State roadways. This study evaluates the requirements for driveway spacing by Table 2-1 and Table 2-2 of the TxDOT Access Management Manual (AMM) as shown in the table below. TxDOT measures driveway spacing along the edge of traveled way from the closest edge of pavement of the first access connection to the closest edge of pavement of the second access connection.

Posted Speed Limit	One-Way Frontage Roads	Two-Way Frontage Roads	Other State Highways
≤ 30 MPH	200'	200'	200'
35 MPH	250'	300'	250'
40 MPH	305'	360'	305'
45 MPH	360'	435'	360'
≥ 50 MPH	425'	510'	425'

**Table 5** summarizes driveway spacing for each access point according to TxDOT’s requirements. Also, driveway spacing results are graphically shown in **Exhibit 2**.

*Table 5. Driveway Spacing Analysis Summary*

PROPOSED DRIVEWAY	SCENARIO	SPACING PROVIDED	SPACING REQUIRED
Avalon Boulevard along FM 407	To the west	590'	425'
	To the east	940'	425'
Drive 1 along FM 407	To the west	940'	425'
	To the east	440'	425'
Gateway Drive along FM 407	To the west	440'	425'
	To the east	655'	425'
Drive 2 along FM 407	To the west	655'	425'
	To the east	600'	425'
Drive 3 along I-35W NBFR	To the south	600'	425'
	To the north	915'	425'
Drive 4 along I-35W NBFR	To the south	915'	425'
	To the north	+1,000'	425'

### Turn Lane Analysis

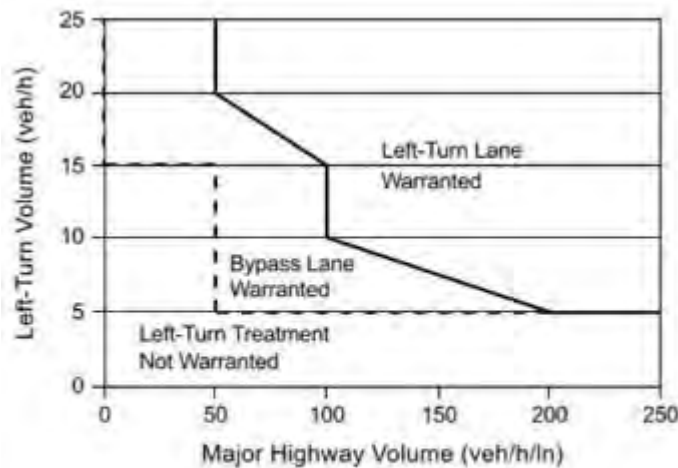
This study evaluates the need for left-turn auxiliary lanes at the study area driveway(s) in accordance with the TxDOT Roadway Design Manual (RDM) Table 3-10, as shown in the table below.

LEFT-TURN LANE PEAK-HOUR VOLUME (VEH./HOUR)	THREE-LEG INTERSECTION MAJOR-ROAD PEAK-HOUR VOLUME (VEH/HR/LN) FOR A LEFT-TURN LANE
5	200
10	100
15	100
20	50
25	50

In order to analyze the requirements for a deceleration lane, calculating the projected peak hour driveway volumes is required. Traffic generated by the proposed development was calculated using the standard approach mentioned above. Supporting data developed for these calculations are provided in **Appendix B**. A summary of the left-turn lane analysis for each of the proposed site access points is presented in **Table 6** and the figure below from the TxDOT RDM Figure 3-8.

Table 6. Left-Turn Lane Analysis Summary

PROPOSED DRIVEWAY		PROJECTED HOURLY VOLUME		THRESHOLD	LANE REQUIRED?
		INBOUND SITE VOLUME (VEH/HR)	THREE-LEG INTERSECTION MAJOR-ROAD PEAK-HOUR VOLUME (VEH/HR/LN) FOR A LEFT-TURN LANE	THREE-LEG INTERSECTION MAJOR-ROAD PEAK-HOUR VOLUME (VEH/HR/LN) FOR A LEFT-TURN LANE	
FM 407, Avalon Boulevard	AM	50	366	50	Yes
	PM	72	529	50	Yes
FM 407, Gateway Drive	AM	77	370	50	Yes
	PM	201	464	50	Yes
FM 407, Drive 2	AM	29	339	50	Yes
	PM	84	429	50	Yes



(a) Three-Leg Intersections

Also, the TxDOT criteria for providing right-turn deceleration lanes are outlined in Table 2-3 of the Access Management Manual. The threshold for roadways with a posted speed limit greater than 45 MPH is 50 vehicles per hour (or, 60 vehicles per hour if the posted speed limit is 45 MPH or less).

In order to analyze the requirements for a right-turn auxiliary lane, calculating the projected peak hour driveway volumes is required. Traffic generated by the proposed development was calculated using the standard approach mentioned above. Supporting data developed for these calculations are provided in **Appendix B**. A summary of the right-turn lane analysis for each of the proposed site access points is presented in **Table 7**.

*Table 7. Right-Turn Lane Analysis Summary*

PROPOSED DRIVEWAY		INBOUND SITE VOLUME (VEH/HR)		LANE REQUIRED?
		PROJECTED	THRESHOLD	
FM 407, Avalon Boulevard	AM	32	50	No
	PM	45	50	No
FM 407, Drive 1	AM	5	50	No
	PM	14	50	No
FM 407, Gateway Drive	AM	38	50	No
	PM	93	50	Yes
FM 407, Drive 2	AM	14	50	No
	PM	42	50	No

## CONCLUSIONS AND MITIGATIONS

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Based upon the analyses presented herein, the resulting findings that follow the Traffic Impact Analysis Guidelines are documented below for all study area intersections along with the development’s driveways. Also, detailed information documenting the findings of the analyses is presented in the **Appendix**.

### Project Background Findings

- Roadway Improvements to I-35W and FM 407 within the project buildout were made known to Spiars at the onset of the traffic study. I-35W will be reconfigured from a 4-lane rural interstate to a six-lane urban freeway with an off-ramp connection to the northbound frontage road just north of FM 407. The interchange with FM 407 will also include a u-turn lane for the southbound frontage road in connection with the northbound frontage road. Additionally, FM 407 will be reconstructed from a 2-lane highway to a four-lane median-divided highway from I-35W to Gateway Drive. These assumptions have been applied to the analysis in the year 2030 and 2035 scenarios.
- In addition to the above improvements, the Town will install traffic signals at the intersections of FM 407 at Avalon Boulevard and FM 407 at Gateway Drive. For purposes of this study, traffic signals have been assumed at these intersections for the No-Build and Build Conditions.

### Level of Service Findings

- All intersections and roadways (FM 407) within the study area operate at “acceptable” overall intersection Levels of Services during the Existing Conditions. With the addition of estimated background traffic growth and estimated site development traffic, the overall intersection Levels of Service are projected to continue to operate at “acceptable” Levels of Service with slight increases to the delays except for the following intersections.
- The intersection of I-35W and the Southbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below.
- The intersection of I-35W and the Northbound Frontage Road operates at an “acceptable” over intersection Level of Service in the Build Peak Hour Conditions however it is recommended the Town and TxDOT evaluate the traffic signal timings for the intersection to optimize traffic operations. Mitigations for the degrading conditions are listed in the table below. Mitigations for the degrading conditions are listed in the table below.

(Continued)...

- The exiting southbound maneuvers at Drive 2 along FM 407 are projected to operate at “unacceptable” Levels of Service in the project Build Conditions in both the AM and PM Peak Hours. These “unacceptable” conditions are typical of other similar unsignalized intersections and driveways along major high-volume roadways such as FM 407 and can only be mitigated with the installation of a traffic signal. However, with the anticipated low traffic volumes exiting the minor approach at the driveway from the development, the warrants for a traffic signal are estimated to not be met. Therefore, no mitigations for the “unacceptable” Levels of Service are recommended.

Site Access Findings

- According to the TxDOT guidelines for the installation of a left-turn deceleration lane, it is recommended that the development install eastbound left-turn deceleration lanes at all driveways with full left-turn access along FM 407. Driveways required to have the installation of a left-turn lane include Avalon Boulevard, Gateway Drive, and Drive 2.
- According to the TxDOT guidelines for the installation of a right-turn deceleration lane, it is recommended that the development install a westbound right-turn deceleration lane at Avalon Boulevard for traffic entering the development.

Traffic Impact Analysis Mitigations Table

Scenario	Year	Intersection/Approach/Peak	Recommendation	Responsible Party	Pro-Rata
Build	2035	FM 407 and I-35W Southbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2035	FM 407 and I-35W Northbound Frontage Road	Traffic Signal Timing Adjustments	City/TxDOT	100%
Build	2028	FM 407 and Avalon Boulevard	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Drive 2	Installation of Eastbound Left-Turn Lane	Development	100%
Build	2028	FM 407 and Gateway Drive	Installation of Westbound Right-Turn Lane	Development	100%

*Appendix A – Existing Data Collection and Information*



## PLANNING AND ZONING COMMISSION STAFF REPORT

### Meeting

**Date:** March 4, 2026

**To:** Chairman and Members of the Planning and Zoning Commission

**From:** Harrison Wicks, Director of Community Development  
Susan Raabe, Senior Planner, AICP, Community Development

**Subject:** Public Hearing - Future Land Use Plan (FLUP) Amendment from Estate Residential 1-acre to Mixed Use

### Purpose:

Hold a public hearing, consider and make a recommendation to Town Council on a Future Land Use Plan amendment (FLUP-26-001) from Estate Residential 1-acre to Mixed Use for a 10.00-acre tract of land out of the S.A. Chambers Survey, Abstract No. 308, being the northern portion of the E. W. Yeck Addition, Lot 1R-1, Town of Argyle, Denton County, Texas.

### Existing Condition of Property:

The property is currently undeveloped with miscellaneous agricultural structures, including accessory structures for livestock shelter or storage. The property is open pasture with several stock ponds and clusters of trees scattered throughout the site. The property is located on the south side of East FM 407 and east of the Waterbrook neighborhood.

### Adjacent Existing Land Uses and FLUP Categories:

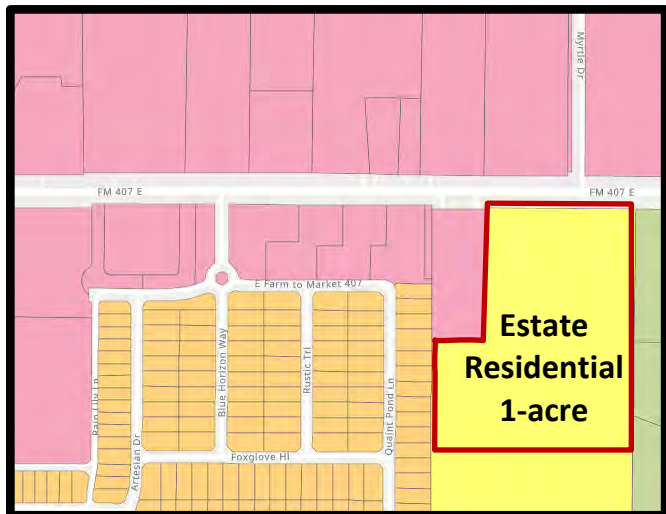
North: Residential and Agriculture; FLUP Mixed-Use  
 South: Residential lots; FLUP Estate Residential (Minimum 1-acre)  
 East: Residential lots; FLUP Ranch Residential (Minimum 2.5-acre)  
 West: Residential and Commercial; FLUP Mixed-Use, and Low to Medium Density Residential

### Development Review Analysis:

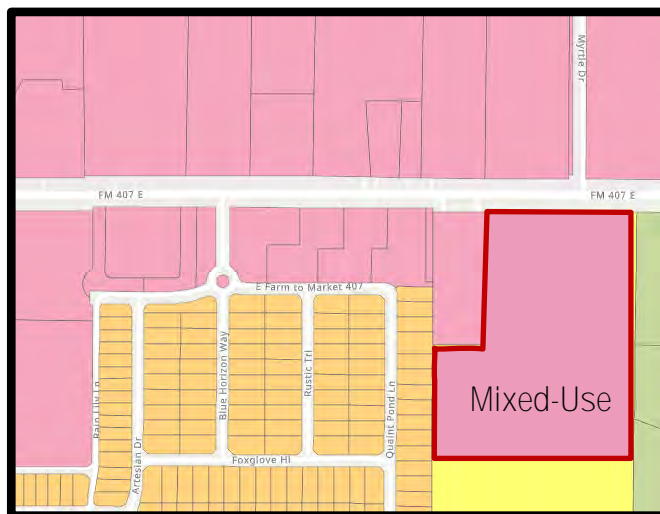
The Town is requesting to amend the Future Land Use Plan a 10-acre tract on East FM 407 W from Estate Residential 1-acre to Mixed-Use. This is for a proposed Small Area Plan for the Town of Argyle designed to accommodate characteristics valued by the Argyle Community, such as public open space, pedestrian use, controlled growth, and natural buffers for surrounding residential neighborhoods.

Mixed-Use is characterized by its flexibility to mix two or more land uses where appropriate. It does not necessarily equate to increased density or residential land uses. Land use is limited to small-scale, pedestrian-oriented commercial, office, and retail uses.

Proximity to current Mixed Use on the north and west side of the property will provide congruent and cohesive future land use and compliment the Residential land use on its lower west and south side.



**Current Future Land Use**



**Proposed Future Land Use**

**Property Owner Notifications:**

Letters were sent out to surrounding property owners within 200’ of the subject property pursuant to the Texas Local Government Code, Subsection 211.006(d). Courtesy letters were also sent out to property owners within 1,000’ of the subject property pursuant to the Town of Argyle Zoning Ordinance and a public hearing sign was posted on the subject property. Staff will provide updates regarding the responses received during the P&Z meeting.

**Staff Recommendation:**

Motion to recommend approval to Town Council of Future Land Use Plan amendment (FLUP-26-001) from Estate Residential 1-acre to Mixed Use on the 10-acre tract of Town Property due to its proximity to current Mixed-Use locations and the flexibility it provides for a Small Area Plan to incorporate elements of public space and conservation of Argyle’s natural environment.

**Attachments:**

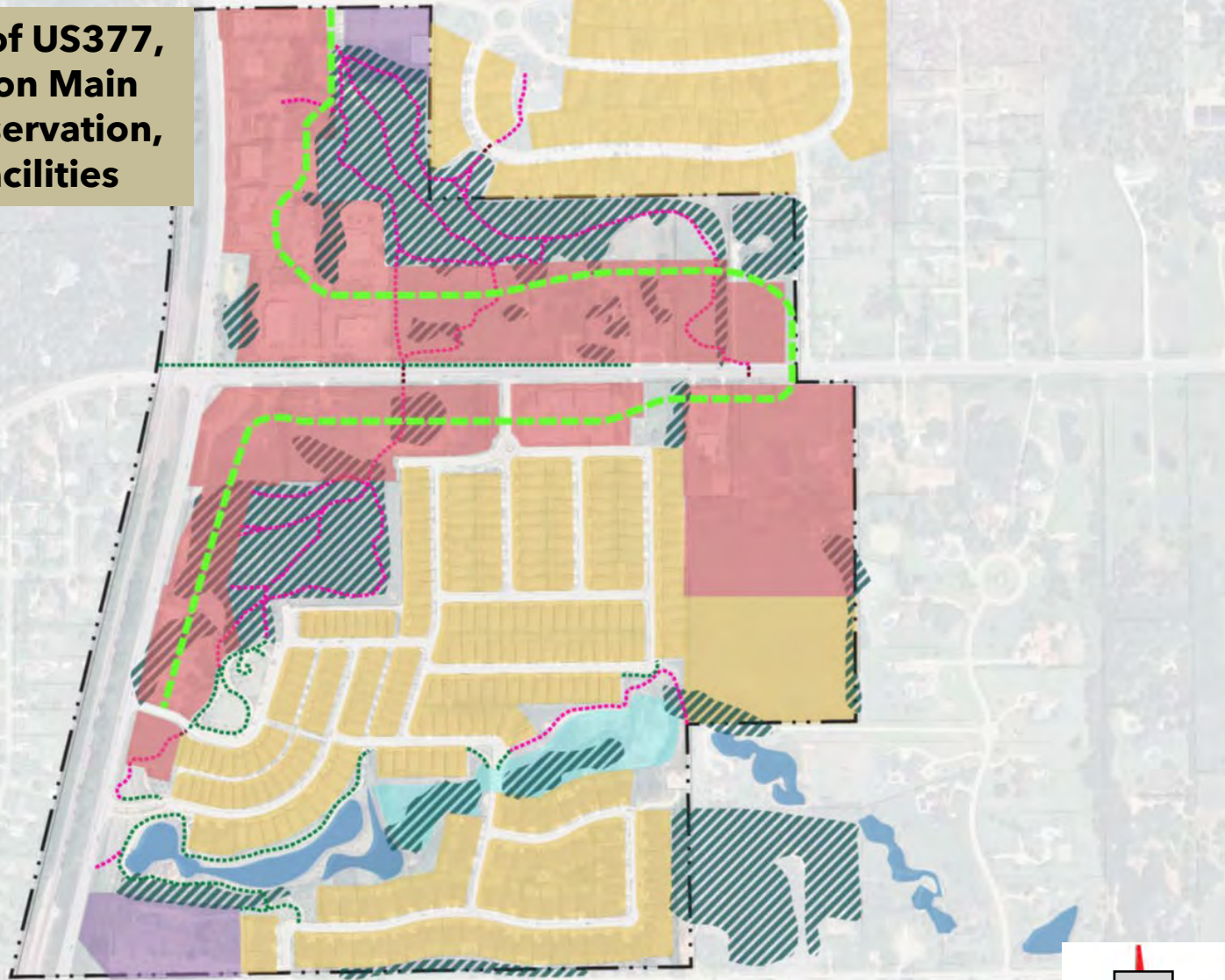
Exhibit A – Town Center District Small Area Plan

# EXHIBIT A TOWN CENTER DISTRICT

Item 5.

FM407 at the eastside of US377,  
planning a green ribbon Main  
Street, linking tree preservation,  
shopping and civic facilities

- MAIN STREET
- TREE COVER
- PROPOSED TRAILS
- EXISTING TRAILS
- CROSS WALKS
- RETAIL/RESTAURANT
- SINGLE-FAMILY RESIDENTIAL
- CHURCHES
- WATER
- FLOODPLAINS



Main Street & Kirk Street Study

ARGYLE NATURE TRAIL

POTENTIAL MAIN STREET ALIGNMENT

US-377

FM 407

POTENTIAL KIRK STREET ALIGNMENT

DETENTION POND





## PLANNING AND ZONING COMMISSION STAFF REPORT

### Meeting

**Date:** March 4, 2026

**To:** Chairman and Members of the Planning and Zoning Commission

**From:** Harrison Wicks, Director of Community Development  
Susan Raabe, Senior Planner, AICP, Community Development

**Subject:** Public Hearing - Zoning Change Request (Z-26-001) from Single Family 2.5-acre Residential Estate (SF-2.5) to Local Retail (LR)

### Purpose:

Hold a public hearing, consider and make a recommendation to Town Council on a Zoning Change Request (Z-26-001) from Single Family 2.5 acre Residential Estate (SF-2.5) to Local Retail (LR) for a 10.00-acre tract of land out of the S.A. Chambers Survey, Abstract No. 308, being the northern portion of the E. W. Yeck Addition, Lot 1R-1, Town of Argyle, Denton County, Texas.

### Existing Condition of Property:

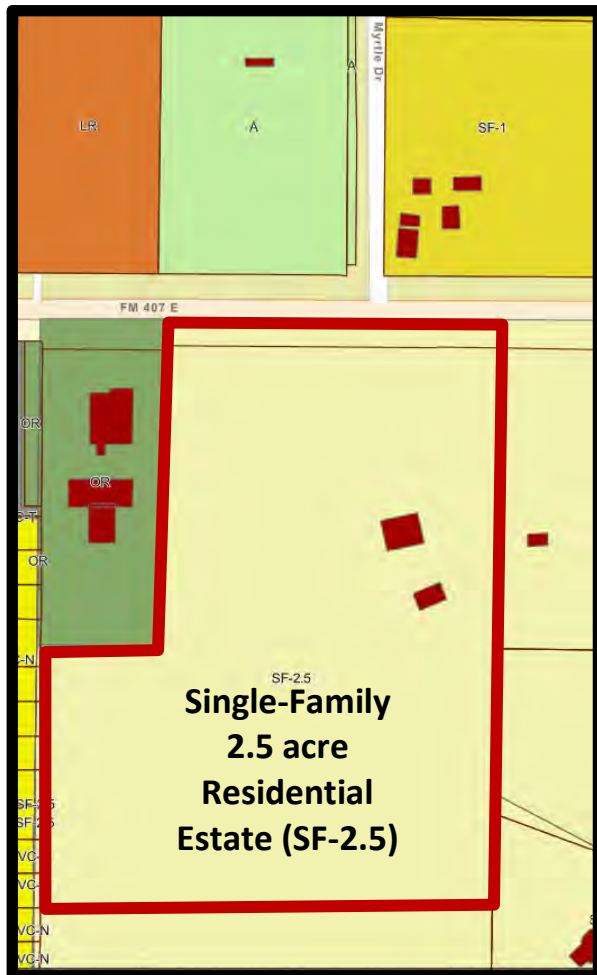
The property is currently undeveloped with miscellaneous agricultural structures, including accessory structures for livestock shelter or storage. The property is open pasture with several stock ponds and clusters of trees scattered throughout the site. The property is located on the south side of East FM 407 and east of the Waterbrook neighborhood.

### Adjacent Existing Land Uses and Zoning:

North: Residential and Agriculture; Local Retail (LR), Agriculture(A), and Single-Family 1 ac (SF-1)  
South: Residential lots; Single-Family 2.5 ac Residential Estate (SF-2.5)  
East: Residential lots; Single-Family 2.5 ac Residential Estate (SF-2.5)  
West: Residential and Commercial; Village Center-Neighborhood (VC-N), Office Retail (OR)

### Development Review Analysis:

The Town is requesting to rezone property on a 10-acre tract on East FM 407 W from Single Family 2.5-acre Residential Estate (SF-2.5) to Local Retail (LR). This is for a proposed Small Area Plan for the Town of Argyle designed to accommodate characteristics valued by the Argyle Community, such as public open space, pedestrian use, controlled growth, and natural buffers for surrounding residential neighborhoods.



**Current Zoning**



**Proposed Zoning**

**Permitted Uses in the Local Retail (LR) Zoning District:**

- Antique Shop (No Outside Sales or Storage)
- Armed Services Recruiting Center
- Art Gallery/Museum/Dealer
- Artisan Workshop/Studio
- Automatic Teller Machines (ATMs)
- Barber Shop/Beauty Shop (Non-College)
- Church/Place of Worship
- Consignment Shop
- Convenience Store without Gas sales
- Dance/Drama/Music Schools (Performing Arts)
- Depository Financial Institution (with Drive-Through)
- Food or Grocery Store

- General Retail, Low-Intensity
- Government Building
- Gymnastic or Dance Studio
- Museum
- Offices, Medical Services
- Pharmacy
- Police Station
- Restaurant (Without Drive-Through)
- School, K through 12 (Public/Private)
- Theater (Non-Motion Picture)
- Used Merchandise; Furniture
- Veterinarian (Indoor Kennels)

**Consistency with Future Land Use Plan:**

A FLUP amendment is required to allow the proposed Local Retail (LR) zoning district. Currently, the FLUP is classified as Estate Residential 1-acre, which does not permit commercial use. The proposed amendment for the FLUP is Mixed-Use.

**Property Owner Notifications:**

Letters were sent out to surrounding property owners within 200' of the subject property pursuant to the Texas Local Government Code, Subsection 211.006(d). Courtesy letters were also sent out to property owners within 1,000' of the subject property pursuant to the Town of Argyle Zoning Ordinance and a public hearing sign was posted on the subject property. Staff will provide updates regarding the responses received during the P&Z meeting.

**Staff Recommendation:**

Motion to recommend approval to Town Council of Zoning Change Request (Z-26-001) from Single Family 2.5-acre Residential Estate (SF-2.5) to Local Retail (LR) on the 10-acre tract of land.

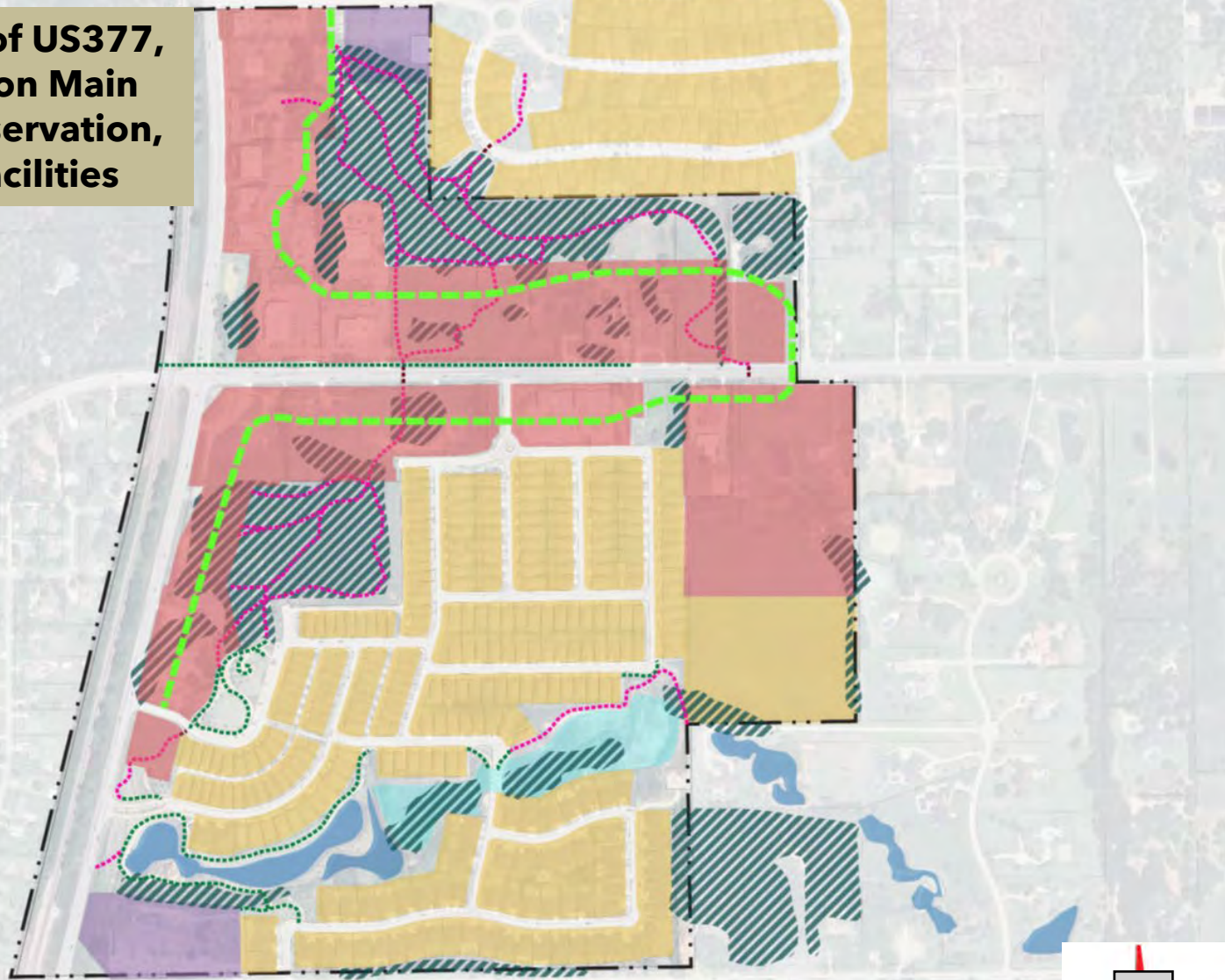
**Attachments:**

Exhibit A – Concept Plan

# EXHIBIT A TOWN CENTER DISTRICT

FM407 at the eastside of US377, planning a green ribbon Main Street, linking tree preservation, shopping and civic facilities

- MAIN STREET
- TREE COVER
- PROPOSED TRAILS
- EXISTING TRAILS
- CROSS WALKS
- RETAIL/RESTAURANT
- SINGLE-FAMILY RESIDENTIAL
- CHURCHES
- WATER
- FLOODPLAINS



Main Street & Kirk Street Study

ARGYLE NATURE TRAIL

POTENTIAL MAIN STREET ALIGNMENT

US-377

FM 407

POTENTIAL KIRK STREET ALIGNMENT

DETENTION POND





## PLANNING AND ZONING COMMISSION STAFF REPORT

**Meeting**

**Date:** March 4, 2026

**To:** Chairman and Members of the Planning and Zoning Commission

**From:** Harrison Wicks, Director of Community Development  
Susan Raabe, Senior Planner, AICP, Community Development

**Subject:** Thoroughfare Plan Modifications

**Purpose:**

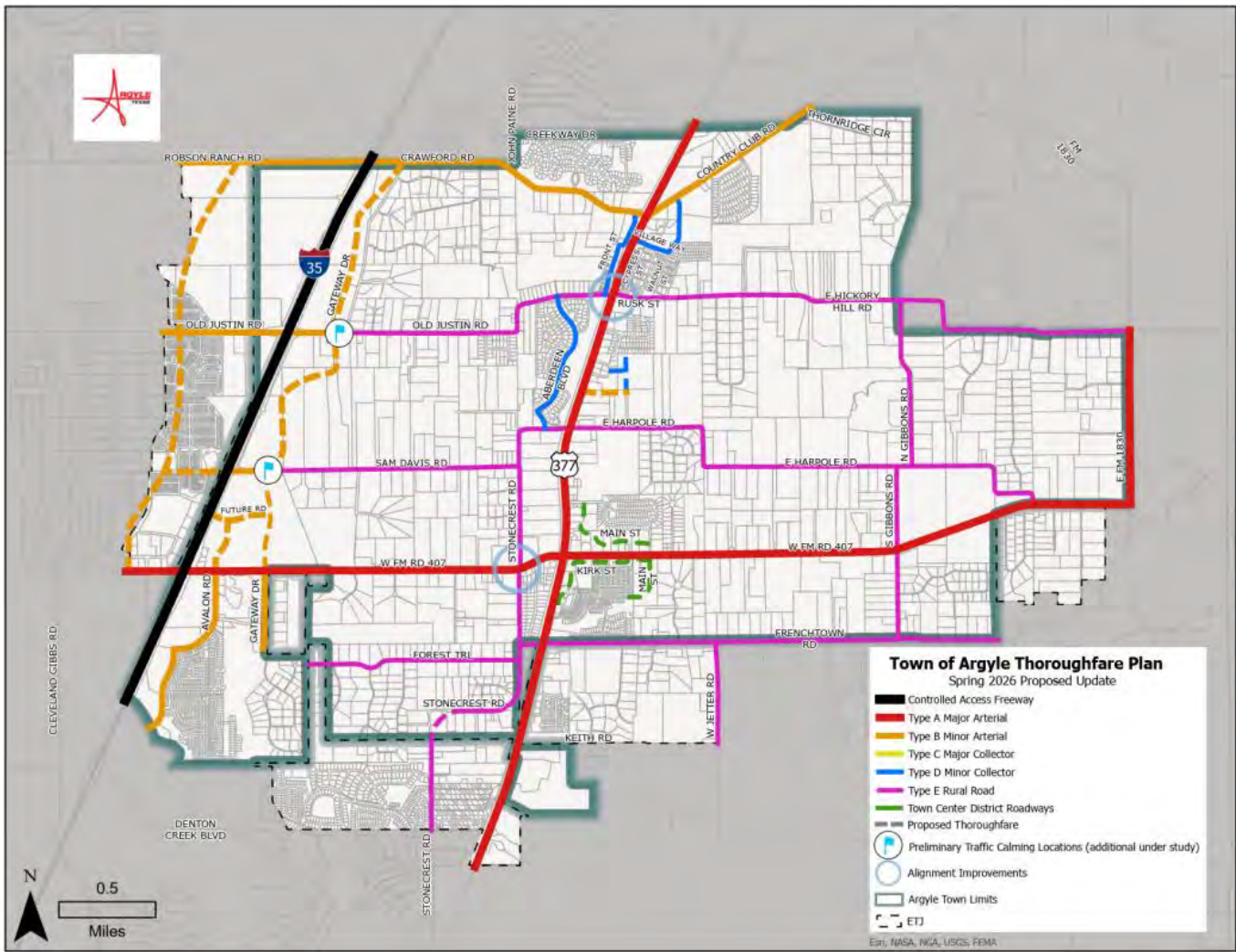
Hold a public hearing, consider and make a recommendation to Town Council on ordinance amendment (FLUP-26-002) to the Thoroughfare Plan of the Town of Argyle Comprehensive Plan adopted June 17, 2024 as Ordinance 2024-17.

**Existing Condition of Thoroughfare Plan:**

The current Thoroughfare Plan requires modification to provide efficient movement and access within the Town of Argyle; and to allow for potential funding of roadway infrastructure through the collection of impact fees. Impact fees are collected from new commercial and residential development in anticipation of future infrastructure expansion or repair from increased demand. In order for Impact Fees funds to be allocated to roadway projects, they must be identified in the Thoroughfare Plan.

**Consistence with Comprehensive Plan:**

The Town of Argyle Thoroughfare Plan provides a framework for the implementation of roadway projects that will accommodate the needs of the community. The Thoroughfare Plan is designed to align with the Comprehensive Plan to ensure a cohesive approach to development and transportation, ensuring that the roadway system supports the overall goals of Argyle.



**Master Thoroughfare Plan Updates:**

- Removed C Taylor Rd as a Type B Minor Arterial
- Added Future Gateway Dr from FM 407 to Crawford Rd
- Added Future Avalon Rd from FM 407 into the Heath Tract
- Added Future Connection from I-35W to Future Avalon Rd
- Extended Avalon Rd to the south through the Avalon at Argyle neighborhood
- Removed the Future extensions of Gateway Dr to the south and Forest Trail to the west
- Added Front St as a Type D Minor Collector
- Added the Future roadway network for potential AISD Middle School
- Updated Aberdeen Blvd and Village Way to a Type D Minor Collector
- Updated Rusk St and the eastern portion of E Harpole Rd to a Type E Rural Road
- Added the Town Center District Roadways, Main St and Kirk St
- Removed Alignment Improvement locations, with the exception of Old Justin at US 377 and Stonecrest Rd at FM 407
- Introduced Preliminary Traffic Calming Locations at the intersections of Old Justin Rd at C Taylor Rd and Sam Davis Rd at Future Gateway Dr
- Removed the Veloweb Shared Use Path
- Updated color for Type D Minor collector from green to blue

**Staff Recommendation:**

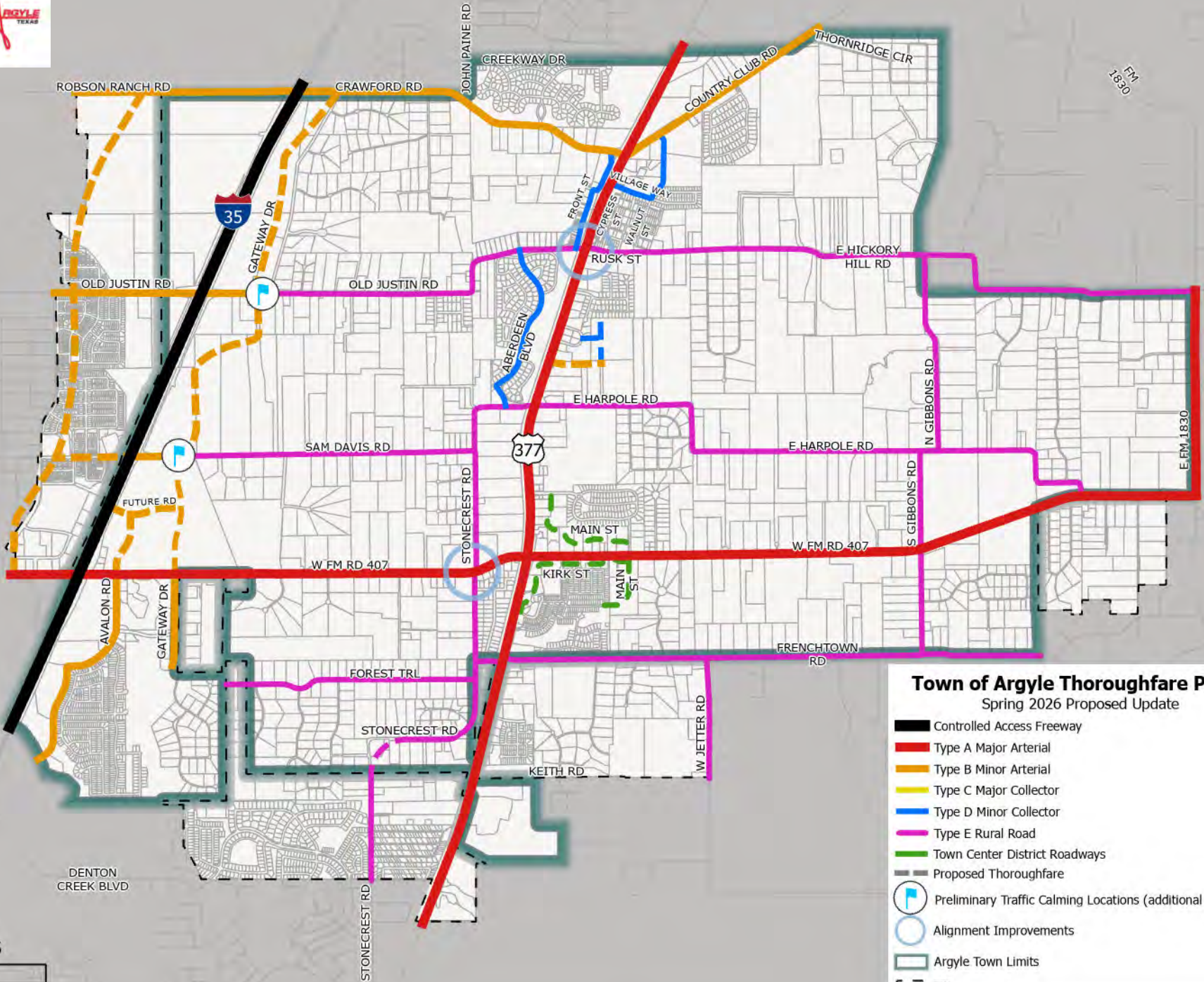
Motion to recommend approval to Town Council of the proposed modifications as presented by Town staff to the Thoroughfare Plan previously adopted June 17, 2024 as Ordinance 2024-17.

**Attachments:**

Exhibit A – Revised Thoroughfare Plan

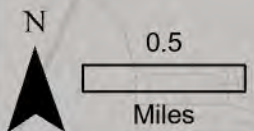
Exhibit B – Current Thoroughfare Plan

# EXHIBIT A



**Town of Argyle Thoroughfare Plan**  
Spring 2026 Proposed Update

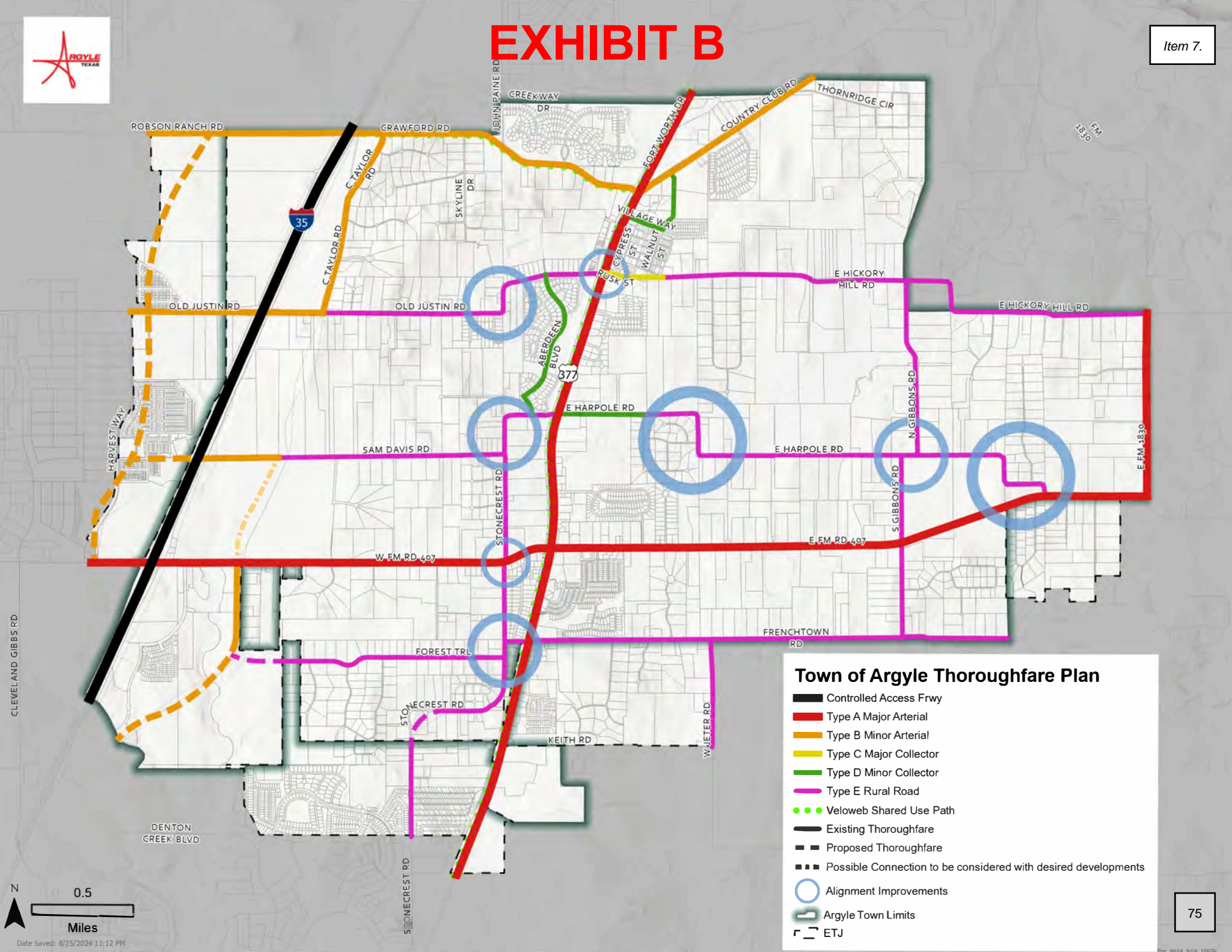
- Controlled Access Freeway
- Type A Major Arterial
- Type B Minor Arterial
- Type C Major Collector
- Type D Minor Collector
- Type E Rural Road
- Town Center District Roadways
- Proposed Thoroughfare
- Preliminary Traffic Calming Locations (additional under study)
- Alignment Improvements
- Argyle Town Limits
- ETJ





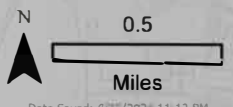
# EXHIBIT B

Item 7.



**Town of Argyle Thoroughfare Plan**

- Controlled Access Frwy
- Type A Major Arterial
- Type B Minor Arterial
- Type C Major Collector
- Type D Minor Collector
- Type E Rural Road
- Veloweb Shared Use Path
- Existing Thoroughfare
- Proposed Thoroughfare
- Possible Connection to be considered with desired developments
- Alignment Improvements
- Argyle Town Limits
- ETJ



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## PLANNING AND ZONING COMMISSION STAFF REPORT

### Meeting

**Date:** March 4, 2026

**To:** Chairman and Members of the Planning and Zoning Commission

**From:** Harrison Wicks, Director of Community Development

**Subject:** Site Plan (SP-26-001) – 606 US Hwy 377 – Akshya Foods, LLC

### Purpose:

Consider and make a recommendation to Town Council on Site Plan request (SP-26-001) for the proposed Akshya Foods, LLC development and restaurant use, located at 606 N US Hwy 377, being on 0.36 acres of land, known as The Original Town of Argyle, Lots 5-10, Block 8, in the J. Davis Survey, Abstract 325, Town of Argyle, Denton County, Texas.

### Existing Condition of Property:

The property is zoned Old Town-1 (OT-1), and fronts on US Hwy 377. One parcel on the site is developed with a 1,300 square foot commercial building, formerly a Real Estate office, and a second parcel to the south that is currently unimproved and vacant.

### Adjacent Existing Land Uses and Zoning:

North: Salon Use, Old Town-1 (OT-1)  
 South: Barbershop Use, Old Town-1 (OT-1)  
 East: Unimproved, Old Town-1 (OT-1)  
 West: US Hwy 377 Right-of-Way

### Background:

The applicant and property owner, Akshya Foods, LLC, purchased the property in December 2025, with the intent of opening a food truck, restaurant use business at the location. In order for this development to be successful they are required to construct a parking lot in order to satisfy off street parking requirements.

### Development Review Analysis:

Review and approval of a site plan is required for site redevelopment or the construction of any new structure within the OT-1 District. Since the applicant is planning to construct a parking lot as part of the project, a site plan application is required.

Town staff has identified this project as a *Restaurant (without Drive-Through)* use, which is defined as “an establishment serving food, including cafeterias, to the general public in specific, designated dining areas or for carry-out for off-premises consumption and shall not include drive-through establishments where food is delivered to or eaten in motor vehicles.”

The applicant has identified an area at the rear of the property where a trailer with a commercial kitchen would serve food, and patrons could either sit in designated indoor and outdoor dining areas. The current 1,300 square foot building on the property would be used for indoor seating, bathrooms for patrons, and storage space. There is no commercial kitchen or other specialized commercial equipment in the building.

**Area Regulations:**

The proposed Site Plan meets all the minimum area regulations of the OT-1 zoning district. Since the property owner is not altering the existing structures on the property, nor constructing new structures, there is no concern of setback or max lot coverage requirements.

**Parking:**

Per Section 14.3.66-6 of the Town Development Standards (TDS), restaurant uses are required to provide one (1) parking space for each 100 square feet of gross-floor area (including indoor/outdoor patio dining areas). When we calculate the 1,300 square feet for the building, and a 400 square foot outdoor seating area, the required number of parking spaces is seventeen (17). The applicant is currently offering 17 parking spaces on the site.

The parking lot is located south of the main building, with direct driveway access to US Hwy 377. The property owner will need to secure a right-of-way permit from TXDOT prior to construction. This will be a condition of approval.

The TDS for the OT-1 zoning district also allows for a special exception to reduce parking spaces. Per Section 14.3.46-4.F-2, the Town may grant a special exception to reduce the required number of parking spaces. In no case shall the required parking be reduced in excess of 25 percent of the required spaces, as set forth herein. Since this applicant meets the parking requirement, this reduction is not needed, but it is available should it ever become needed.

**Tree Preservation:**

A tree survey was not provided. There are currently no plans to remove trees on the site. If the property owner would like to remove trees for any reason, a tree survey will be required.

**Landscaping:**

The project is required to provide a Type B landscape buffer along the front property line along US Hwy 377. The Type B buffer yard is 10 feet wide and consists of two (2) canopy trees, three (3) understory trees and ten (10) shrubs per one hundred linear feet (100'). With 140 Linear Feet, below is an analysis of what is required versus what is provided:

REQUIRED: MINIMUM 10 FT WIDTH

PROVIDED: MINIMUM 10 FT WIDTH

REQUIRED: 3 CANOPY TREES [140 LF / 100 LF (X 2)]

PROVIDED: 5 CANOPY TREES

REQUIRED: 5 UNDERSTORY TREES [140 LF / 100 LF (X 3)]

PROVIDED: 9 UNDERSTORY TREES

REQUIRED: 14 SHRUBS [140 LF / 100 LF (X 10)]

PROVIDED: 23 SHRUBS

The applicant has provided the full Type B landscape buffer as required by the TDS along the US Hwy 377. The OT-1 zoning district standards also requires an additional masonry wall measuring three feet in height and constructed of like and similar masonry materials to those of the primary structure shall be erected along the interior of the landscaped edge. The property owner has proposed installing these masonry walls. No variance is requested.

### **Signage**

The applicant intends to have signage as part of the site but has not yet submitted a sign package to be approved by the Town. The signage demonstrated on the building elevations is conceptual at this time. Prior to the installation of signage, the applicant will be required to submit a sign package demonstrating compliance with Article IV-Sign Regulations of the TDS.

### **Infrastructure Adequacy:**

Water utilities and sanitary sewer are available to this site with sufficient capacity to serve the development. If approved, full civil construction plans for pavement, drainage, water and sanitary sewer are required for submission during the permitting and construction plan review.

### **Staff Recommendation:**

The proposed site plan application meets all the requirements of the Town Development Standards and no variances are requested. Town staff recommend approval of this site plan application with the following condition:

1. That the applicant receive an approved TXDOT right-of-way permit prior to approval of the construction plans.

### **Attachments:**

Exhibit A – Site Development Plans

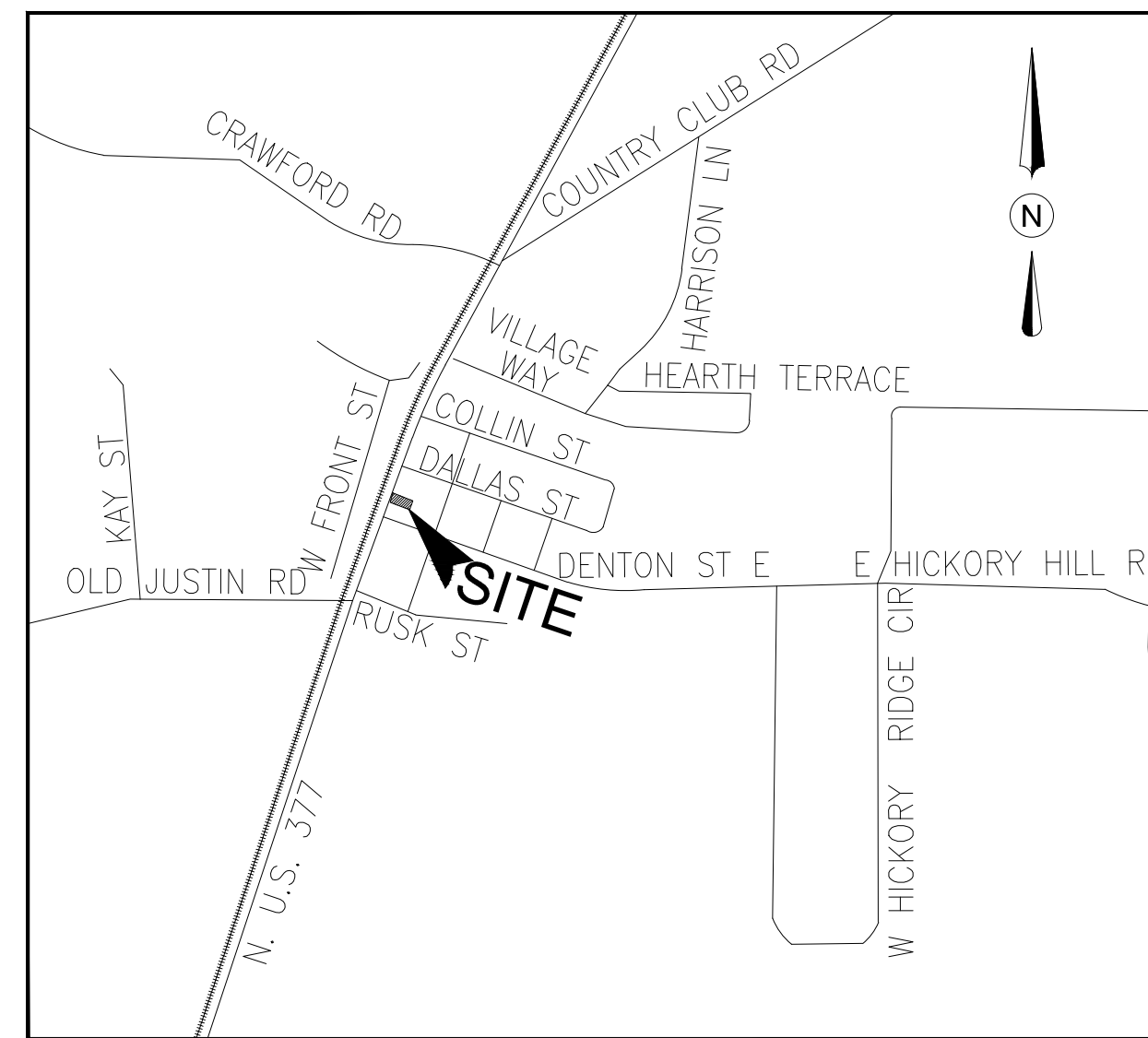
Exhibit B – Renderings

Exhibit C – Floor Plan

# EXHIBIT A

# SITE DEVELOPMENT PLANS FOR FOOD TRUCK 0.18 ACRES (7,875 SQ. FT.) 606 N. U.S. 377, TOWN OF ARGYLE DENTON, TEXAS 76226

PROJECT CONTACT LIST	
ENGINEER SREENIVASULU BOLLU, PE 1132 HARDY DRIVE COVINGTON, LA 70433 SBOLLU@HY-CONSULTING.COM TEL. (985) 951-0413	SURVEYOR THOMAS WILLIAM MAUK ARTHUR LAND SURVEYING 220 ELMST., #200 - LEWISVILLE TX 75057 ARTHURSURVEYING.COM 972-221-9439



VICINITY MAP  
N.T.S.

SHEET LIST TABLE	
C-1.0	COVER SHEET
	SURVEY
C-1.1	GENERAL NOTES
C-2.0	DEMOLITION PLAN
C-3.0	SITE PLAN & LANDSCAPE PLAN
C-3.1	DIMENSION CONTROL PLAN
C-4.0	GRADING PLAN
C-5.0	DRAINAGE PLAN
C-6.0	EROSION CONTROL PLAN
C-6.1	EROSION CONTROL DETAILS
C-7.0	PAVING PLAN
C-7.1	PAVING DETAILS
C-8.0	UTILITY PLAN
C-8.1	UTILITY DETAILS
C-9.0	ADA PLAN AND DETAILS
C-10.0	LIGHTING PLAN



CADD FILE NAME:	
BASE	
APP'D	
REVISIONS	
REMARKS	
DATE	



**HY ENGINEERING LLC**  
 1132 HARDY DRIVE  
 COVINGTON, LA 70433  
 FIRM #26178  
 SBOLLU@HY-CONSULTING.COM  
 985-951-0413

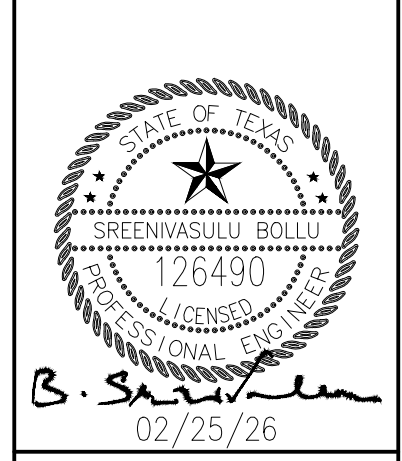
SCALE: 36X24	DESIGNED BY: S.BOLLU	PROJECT NO: 2026-101
1"=10'	CHECKED BY: S.BOLLU	DATE: FEB 2026
DRAWN BY: S.BOLLU		

**COVER SHEET**  
**FOOD TRUCK**  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226

SHEET NO.	<b>C-1.0</b>
1	OF 15

**BASE**

APP'D	DATE	REVISIONS	REMARKS



**HY ENGINEERING LLC**  
 1132 HARDY DRIVE  
 COVINGTON, LA 70433

FIRM #26178  
 SBOLLU@HY-CONSULTING.COM

DESIGNED BY:	S.BOLLU
CHECKED BY:	S.BOLLU
PROJECT NO.:	2026-101

SCALE:	36X24
DATE:	FEB 2026

**GENERAL NOTES**  
**FOOD TRUCK**  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226

SHEET NO. **C-1.1**



**UTILITY GENERAL NOTES**

- ALL CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE TOWN/UTILITY COMPANY STANDARDS.
- FIELD VERIFY LOCATION OF EXISTING WATER MAIN, SEWER MAIN, GAS, TELEPHONE AND ELECTRICAL LINE. POT HOLE RECOMMENDED PRIOR TO CONSTRUCTION BEGIN. CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE WITH UTILITY SERVICE PROVIDERS.
- THE LOCATION OF UNDERGROUND UTILITIES INDICATED ON THE PLANS IS TAKEN FROM AS-BUILTS, UTILITY PLANS OR SURVEY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND UTILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL UTILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND UTILITIES. IF EXISTING UNDERGROUND UTILITIES ARE DAMAGED, THE CONTRACTOR WILL BE RESPONSIBLE FOR THE COST OF REPAIRING THE UTILITY.
- WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL REPLACE OR REPAIR THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, UNLESS OTHERWISE SHOWN OR NOTED ON THE PLANS, AT HIS OWN COST AND EXPENSE. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER AT ONCE OF ANY CONFLICTS WITH UTILITIES.
- ALL EXCAVATIONS, TRENCHING AND SHORING OPERATIONS SHALL COMPLY WITH THE REQUIREMENTS OF THE U. S. DEPARTMENT OF LABOR, OSHA, CONSTRUCTION SAFETY AND HEALTH REGULATIONS AND ANY AMENDMENTS THERETO.
- ADEQUATE MEASURES SHALL BE TAKEN TO PREVENT EROSION. IN THE EVENT THAT SIGNIFICANT EROSION OCCURS AS A RESULT OF CONSTRUCTION THE CONTRACTOR SHALL RESTORE THE ERODED AREA TO ORIGINAL CONDITION OR BETTER.
- THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION TO ORIGINAL CONDITION OR BETTER. RESTORED AREAS INCLUDE, BUT ARE NOT LIMITED TO TRENCH BACKFILL, SIDE SLOPES, FENCES, CULVERT PIPES, DRAINAGE DITCHES, DRIVEWAYS, PRIVATE YARDS AND ROADWAYS.
- ANY CHANGES NEEDED AFTER CONSTRUCTION PLANS HAVE BEEN RELEASED, SHALL BE APPROVED BY THE TOWN ENGINEER. THESE CHANGES MUST BE RECEIVED IN WRITING.
- THE CONTRACTOR SHALL PROVIDE "RED LINED" MARKED PRINTS TO THE ENGINEER PRIOR TO FINAL INSPECTION INDICATING ALL CONSTRUCTION WHICH DEVIATED FROM THE PLANS OR WAS CONSTRUCTED IN ADDITION TO THAT INDICATED ON THE PLANS.

CONTRACTOR SHALL ENSURE THAT ALL REQUIRED PERMITS ARE OBTAINED AND APPROVALS FROM ALL GOVERNING AGENCIES ARE RECEIVED PRIOR TO START OF CONSTRUCTION.

**SITE GENERAL NOTES**

- ALL CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE TOWN OR LOCAL JURISDICTION STANDARDS.
- THE LOCATION OF UNDERGROUND UTILITIES INDICATED ON THE PLANS IS TAKEN FROM AS-BUILTS, UTILITY PLANS OR SURVEY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND UTILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL UTILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND UTILITIES. IF EXISTING UNDERGROUND UTILITIES ARE DAMAGED, THE CONTRACTOR WILL BE RESPONSIBLE FOR THE COST OF REPAIRING THE UTILITY.
- WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL REPLACE OR REPAIR THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, UNLESS OTHERWISE SHOWN OR NOTED ON THE PLANS, AT HIS OWN COST AND EXPENSE. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER AT ONCE OF ANY CONFLICTS WITH UTILITIES.
- ALL EXCAVATIONS, TRENCHING AND SHORING OPERATIONS SHALL COMPLY WITH THE REQUIREMENTS OF THE U. S. DEPARTMENT OF LABOR, OSHA, CONSTRUCTION SAFETY AND HEALTH REGULATIONS AND ANY AMENDMENTS THERETO.
- THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION TO ORIGINAL CONDITION OR BETTER. RESTORED AREAS INCLUDE, BUT ARE NOT LIMITED TO TRENCH BACKFILL, SIDE SLOPES, FENCES, DRAINAGE DITCHES, DRIVEWAYS, PRIVATE YARDS AND ROADWAYS.
- ANY CHANGES NEEDED AFTER CONSTRUCTION PLANS HAVE BEEN RELEASED, SHALL BE APPROVED BY THE CITY ENGINEER. THESE CHANGES MUST BE RECEIVED IN WRITING.
- THE CONTRACTOR SHALL PROVIDE "RED LINED" MARKED PRINTS TO THE ENGINEER PRIOR TO FINAL INSPECTION INDICATING ALL CONSTRUCTION WHICH DEVIATED FROM THE PLANS OR WAS CONSTRUCTED IN ADDITION TO THAT INDICATED ON THE PLANS.

**PAVING GENERAL NOTES**

- STRIP & REMOVE FROM THE CONSTRUCTION AREA ALL TOPSOIL, ORGANICS & VEGETATION TO A MINIMUM DEPTH OF 6 INCHES.
- FRENCH DRAINS ARE RECOMMENDED AROUND ANY SLABS WHERE SEEPING GROUND WATER IS ENCOUNTERED DURING CONSTRUCTION.
- THE CONTRACTOR SHALL PROCEED WITH PAVING NO MORE THAN SEVENTY-TWO (72) HOURS AFTER DENSITY/MOISTURE TESTS HAVE BEEN TAKEN AND PASSED BY A REGULAR TESTING FIRM.
- MANHOLE RIM ELEVATIONS, CLEAN-OUTS, VALVE BOXES, ETC. SHALL BE ADJUSTED TO FINISHED GRADE BY THE PAVING CONTRACTOR AT THE TIME OF PAVING.

**GRADING GENERAL NOTES**

- ALL SURPLUS EXCAVATION AND WASTE MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND IT SHALL BE HIS SOLE RESPONSIBILITY TO REMOVE SUCH SURPLUS EXCAVATION AND WASTE MATERIAL FROM THE SITE TO A PUBLIC DUMP SITE APPROVED FOR THE DISPOSAL OF SUCH MATERIALS. IF SURPLUS EXCAVATION IS REMOVED FROM THIS SITE TO ANOTHER PROPERTY, IT SHALL BE PLACED ON SUCH PROPERTY WITH THE WRITTEN CONSENT OF THE OWNER(S) OF SUCH PROPERTY. A COPY OF SUCH WRITTEN CONSENT SHALL BE PROVIDED TO THE OWNER. IF THE CONTRACTOR WISHES TO DISPOSE OF SURPLUS EXCAVATION ON-SITE, IT SHALL BE ONLY WITH THE PRIOR APPROVAL OF THE OWNERS PROJECT REPRESENTATIVE AND CARE SHOULD BE TAKEN TO AVOID BLOCKING NATURAL DRAINAGE AND INCREASING STEEP SLOPES. IF ANY OF THE HAULED EXCAVATION MATERIAL IS TAKEN TO ANOTHER LOCATION WITHIN THE CITY LIMITS, THE OWNER OF THE PROPERTY IS REQUIRED TO OBTAIN A LOT GRADING PERMIT BEFORE MATERIAL IS DELIVERED.
- THE CONTRACTOR IS REQUIRED TO PROVIDE HIS OWN STAKING AND TO VERIFY PROJECT ELEVATIONS. "MATCH EXISTING" SHALL BE UNDERSTOOD TO APPLY TO BOTH VERTICAL ELEVATION AND HORIZONTAL ALIGNMENT.
- THE CONTRACTOR SHALL PREPARE ALL LANDSCAPE AREAS INCLUDING STREET RIGHT-OF-WAY AREAS TO AN ACCEPTABLE SUBGRADE CONDITION IN ACCORDANCE WITH THE LANDSCAPE PLANS. IF THE CONTRACTOR IS NOT EMPLOYED TO PROVIDE AND INSTALL LANDSCAPING, HE SHALL PREPARE A FINISHED AND COMPACTED SUB-GRADE IN THE LANDSCAPING AREAS.
- NO SLOPES TO EXCEED 3H:1V WITHOUT SLOPE STABILIZATION.

**DEMOLITION GENERAL NOTES**

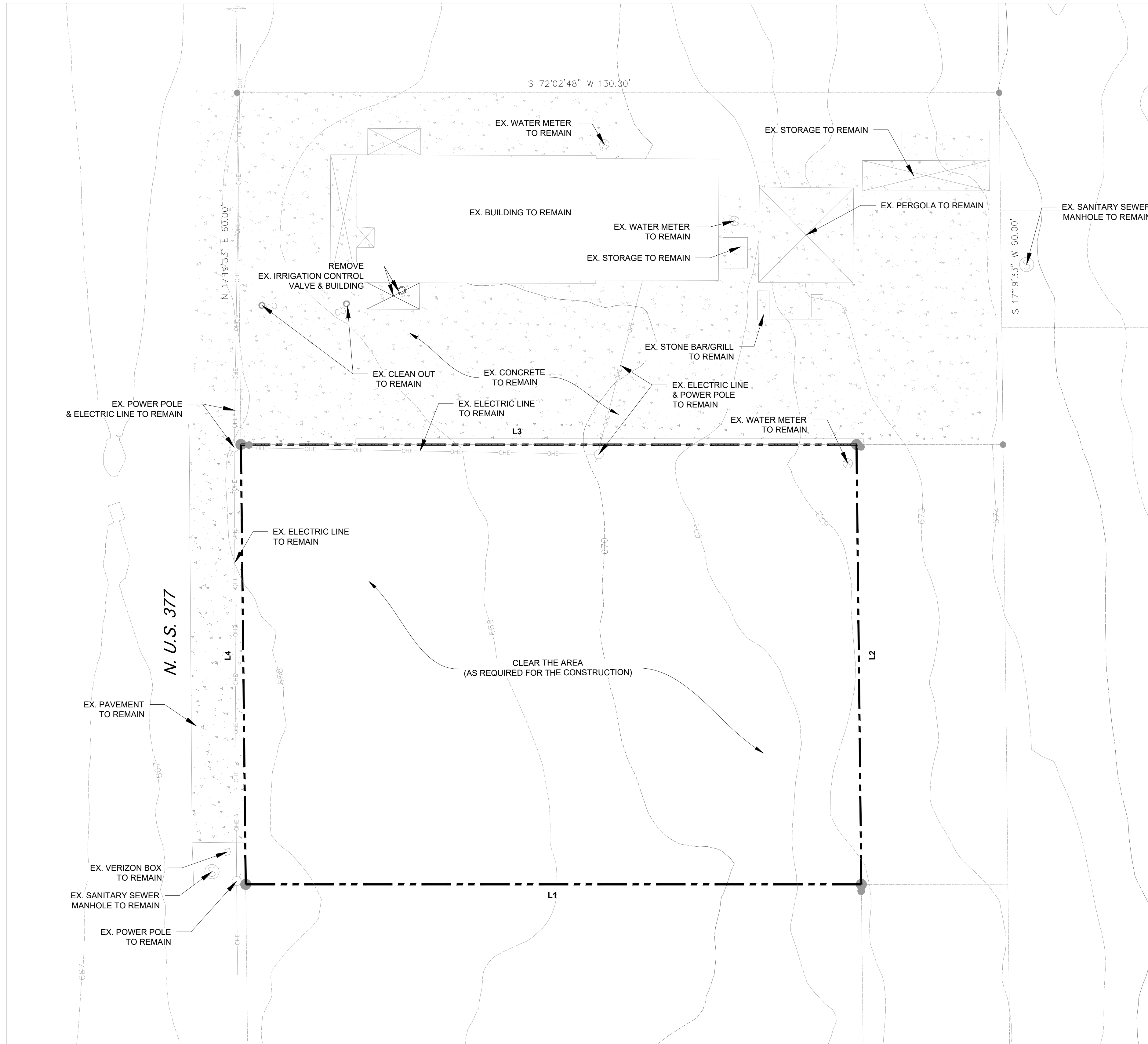
- ANY DEMOLITION IS TO BE PERFORMED IN STRICT CONFORMANCE WITH ALL APPLICABLE TOWN, COUNTY AND STATE, AND/OR GOVERNING BODY'S STANDARDS.
- EROSION AND SEDIMENT CONTROL MEASUREMENTS SHALL BE MAINTAINED AT ALL TIMES DURING DEMOLITION.
- THE PURPOSE OF THIS DRAWING IS TO CONVEY THE OVERALL SCOPE OF WORK AND IT IS NOT INTENDED TO COVER ALL DETAILS OR SPECIFICATIONS REQUIRED TO COMPLY WITH GENERALLY ACCEPTED DEMOLITION PRACTICES. CONTRACTOR SHALL THOROUGHLY GET FAMILIARIZED WITH THE SITE, SCOPE OF WORK, AND ALL EXISTING CONDITIONS AT THE JOB SITE PRIOR TO BIDDING AND COMMENCING THE WORK. THE DEMOLITION CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR MEANS, METHODS, TECHNIQUES, OR PROCEDURES USED TO COMPLETE THE WORK IN ACCORDANCE WITH THE CONSTRUCTION DOCUMENTS AND IS LIABLE FOR THE SAFETY OF THE PUBLIC OR CONTRACTOR'S EMPLOYEES DURING THE COURSE OF THE PROJECT.
- THE DEMOLITION PLAN IS INTENDED TO SHOW REMOVAL OF KNOWN SITE FEATURES AND UTILITIES AS SHOWN ON THE SURVEY. THERE MAY BE OTHER SITE FEATURES, UTILITIES, STRUCTURES, AND MISCELLANEOUS ITEMS BOTH BURIED AND ABOVE GROUND THAT ARE WITHIN THE LIMITS OF WORK THAT MAY NEED TO BE REMOVED FOR THE PROPOSED PROJECT THAT ARE NOT SHOWN HEREON. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE CITY, ENGINEER AND/OR OWNER PRIOR TO REMOVING ITEMS NOT SHOWN ON THE PLANS.
- THE CONTRACTOR SHALL CONTACT RESPECTIVE UTILITY COMPANIES PRIOR TO DEMOLITION TO COORDINATE DISCONNECTION AND REMOVAL OF EXISTING UTILITIES WITHIN THE AREA OF WORK.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES THAT ARE INTENDED TO CONTINUE TO PROVIDE SERVICE WHETHER THESE UTILITIES ARE SHOWN ON THE PLAN OR NOT.
- UPON DISCOVERY OF ANY UNDERGROUND TANKS, CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE. NO REMOVAL OF TANKS SHALL OCCUR UNTIL AUTHORIZED BY OWNER.
- BUILDING AND APPURTENANCES DESIGNATED FOR DEMOLITION SHALL NOT BE DISTURBED BY THE CONTRACTOR UNTIL HE HAS BEEN FURNISHED WITH NOTICE TO PROCEED BY THE OWNER. AS SOON AS SUCH NOTICE HAS BEEN GIVEN, THE CONTRACTOR SHALL PERFORM THE DEMOLITION, UNDER THE DIRECTION OF THE OWNER'S REPRESENTATIVE.
- DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL UNSUITABLE MATERIAL AND DEBRIS SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN ACCORDANCE WITH ALL CITY, STATE, AND FEDERAL LAWS AND ORDINANCES.
- AS SOON AS DEMOLITION WORK HAS BEEN COMPLETED, THE FINAL GRADE OF BACKFILL IN DEMOLITION AREAS SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT. CONTRACTOR TO PREVENT WATER FROM DRAINING ONTO ADJACENT PROPERTIES.
- EXISTING TREES TO REMAIN SHOULD BE PROTECTED FROM DAMAGE DURING DEMOLITION AND CONSTRUCTION.

**EROSION CONTROL GENERAL NOTES**

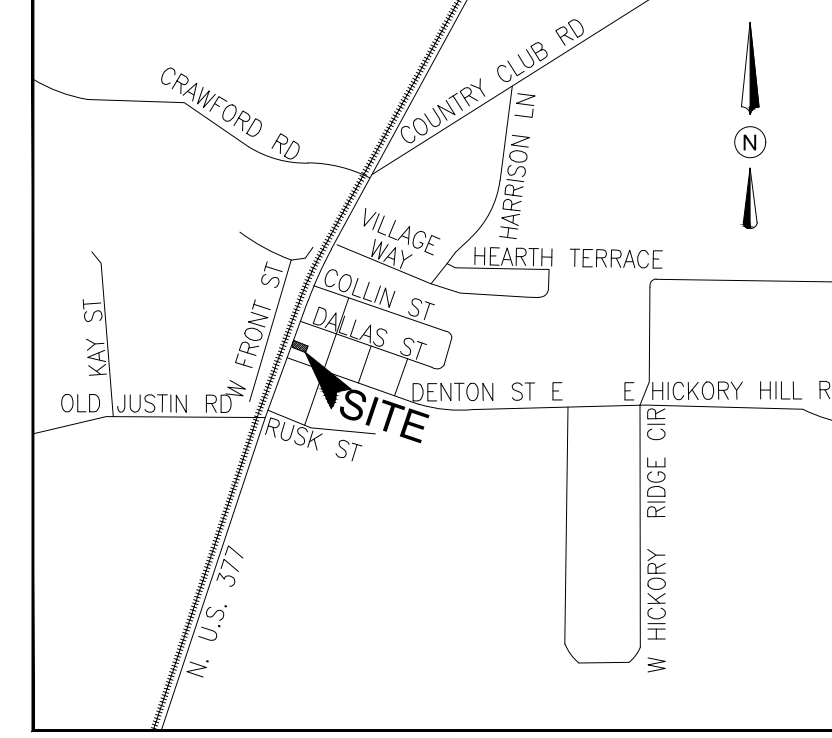
- EVERY SOIL DISTURBING ACTIVITY SHALL HAVE AN ACCOMPANYING EROSION CONTROL PLAN.
- THE CONTRACTOR SHALL COMPLY WITH THE TOWN'S STORM WATER ORDINANCE, THE TPDES GENERAL CONSTRUCTION PERMIT TXR150000 AND ANY OTHER STATE AND/OR LOCAL REGULATIONS.
- THE SITE SHALL BE INSPECTED BY THE CONTRACTOR OR HIS REPRESENTATIVE WEEKLY, AND AFTER ANY MAJOR STORM ADJUSTMENTS/REPAIRS TO THE EROSION CONTROL MEASURES SHOULD BE MADE AS NEEDED.
- CONTRACTOR SHALL VEGETATE ALL DISTURBED AREAS IMMEDIATELY UPON COMPLETION OF GRADING ACTIVITIES. FINAL ACCEPTANCE OF A SITE SHALL BE CONTINGENT UPON VEGETATION BEING ESTABLISHED IN ALL DISTURBED AREAS.
- ADEQUATE MEASURES SHALL BE TAKEN TO PREVENT EROSION. IN THE EVENT THAT SIGNIFICANT EROSION OCCURS AS A RESULT OF CONSTRUCTION THE CONTRACTOR SHALL RESTORE THE ERODED AREA TO ORIGINAL CONDITION OR BETTER.
- TEMPORARY STONE STABILIZED CONSTRUCTION ENTRANCE SHALL HAVE THE FOLLOWING MINIMUM DIMENSIONS: 15' WIDE X 50' LONG X 6" DEEP. (2'-5" COARSE AGGREGATE). PLACE FILTER FABRIC UNDER STONE.
- THE CONCRETE WASHOUT AREA IS TO BE USED AS A VEHICLE WASH DOWN AREA FOR DEBRIS AND SOIL REMOVAL PRIOR TO EXITING THE SITE.

**ADA GENERAL NOTES**

- SLOPE AND CROSS SLOPE OF A SIDEWALK LEADING INTO A CURB RAMP'S SYSTEM SHALL BE A MAXIMUM OF 1:20 (5%) AND 1:50 (2%) CROSS SLOPE.
- MAXIMUM SLOPE OF A HANDICAPPED ACCESSIBLE ROUTE ALONG A SIDEWALK LEADING INTO A CURB RAMP, BEFORE IT MUST BE CONSIDERED A RAMP IS 1:20 (5%) AND 1:50 (2%) CROSS SLOPE.
- MAXIMUM SLOPE AND CROSS SLOPE OF HANDICAPPED ACCESSIBLE PARKING SPACE & ADJACENT ACCESS AISLE IS 1:50 (2%) IN ANY DIRECTION.



EXISTING LEGEND	
	BOUNDARY
	OVER-HEAD-ELECTRIC LINE
	EXISTING MINOR CONTOURS
	EXISTING MAJOR CONTOURS
	ADJACENT BOUNDARY
	CONCRETE PAVEMENT
	CONCRETE
	IRON ROD
	POWER POLE
	WATER METER
	SANITARY SEWER MANHOLE
	VERIZON BOX
	CLEAN OUT
	IRRIGATION CONTROL VALVE



VICINITY MAP  
N.T.S.

Line Table		
Line #	Length	Direction
L1	105'	N72° 02' 48"W
L2	75'	S17° 19' 33"W
L3	105'	S72° 02' 48"W
L4	75'	N17° 19' 33"E

DEMOLITION LEGEND	
	DEMO AREA

DEMOLITION GENERAL NOTES

1. ANY DEMOLITION IS TO BE PERFORMED IN STRICT CONFORMANCE WITH ALL APPLICABLE TOWN, COUNTY AND STATE, AND/OR GOVERNING BODY'S STANDARDS.
2. EROSION AND SEDIMENT CONTROL MEASUREMENTS SHALL BE MAINTAINED AT ALL TIMES DURING DEMOLITION.
3. THE PURPOSE OF THIS DRAWING IS TO CONVEY THE OVERALL SCOPE OF WORK AND IT IS NOT INTENDED TO COVER ALL DETAILS OR SPECIFICATIONS REQUIRED TO COMPLY WITH GENERALLY ACCEPTED DEMOLITION PRACTICES. CONTRACTOR SHALL THOROUGHLY GET FAMILIARIZED WITH THE SITE, SCOPE OF WORK, AND ALL EXISTING CONDITIONS AT THE JOB SITE PRIOR TO BIDDING AND COMMENCING THE WORK. THE DEMOLITION CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR MEANS, METHODS, TECHNIQUES, OR PROCEDURES USED TO COMPLETE THE WORK IN ACCORDANCE WITH THE CONSTRUCTION DOCUMENTS AND IS LIABLE FOR THE SAFETY OF THE PUBLIC OR CONTRACTOR'S EMPLOYEES DURING THE COURSE OF THE PROJECT.
4. THE DEMOLITION PLAN IS INTENDED TO SHOW REMOVAL OF KNOWN SITE FEATURES AND UTILITIES AS SHOWN ON THE SURVEY. THERE MAY BE OTHER SITE FEATURES, UTILITIES, STRUCTURES, AND MISCELLANEOUS ITEMS BOTH BURIED AND ABOVE GROUND THAT ARE WITHIN THE LIMITS OF WORK THAT MAY NEED TO BE REMOVED FOR THE PROPOSED PROJECT THAT ARE NOT SHOWN HEREON. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE TOWN, ENGINEER AND/OR OWNER PRIOR TO REMOVING ITEMS NOT SHOWN ON THE PLANS.
5. THE CONTRACTOR SHALL CONTACT RESPECTIVE UTILITY COMPANIES PRIOR TO DEMOLITION TO COORDINATE DISCONNECTION AND REMOVAL OF EXISTING UTILITIES WITHIN THE AREA OF WORK.
6. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING UTILITIES THAT ARE INTENDED TO CONTINUE TO PROVIDE SERVICE WHETHER THESE UTILITIES ARE SHOWN ON THE PLAN OR NOT.
7. UPON DISCOVERY OF ANY UNDERGROUND TANKS, CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE. NO REMOVAL OF TANKS SHALL OCCUR UNTIL AUTHORIZED BY OWNER.
8. BUILDING AND APPURTENANCES DESIGNATED FOR DEMOLITION SHALL NOT BE DISTURBED BY THE CONTRACTOR UNTIL HE HAS BEEN FURNISHED WITH NOTICE TO PROCEED BY THE OWNER. AS SOON AS SUCH NOTICE HAS BEEN GIVEN, THE CONTRACTOR SHALL PERFORM THE DEMOLITION, UNDER THE DIRECTION OF THE OWNER'S REPRESENTATIVE.
9. DEBRIS SHALL NOT BE BURIED ON THE SUBJECT SITE. ALL UNSUITABLE MATERIAL AND DEBRIS SHALL BE REMOVED FROM THE SITE AND DISPOSED OF IN ACCORDANCE WITH ALL TOWN, STATE, AND FEDERAL LAWS AND ORDINANCES.
10. AS SOON AS DEMOLITION WORK HAS BEEN COMPLETED, THE FINAL GRADE OF BACKFILL IN DEMOLITION AREAS SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT. CONTRACTOR TO PREVENT WATER FROM DRAINING ONTO ADJACENT PROPERTIES.
11. EXISTING TREES TO REMAIN SHOULD BE PROTECTED FROM DAMAGE DURING DEMOLITION AND CONSTRUCTION.
12. APPROVAL FROM THE CHIEF ARBORIST IS REQUIRED BEFORE ANY TREES MAY BE REMOVED DURING DEMOLITION OR CONSTRUCTION.



Know what's below.  
Call before you dig.



HY ENGINEERING LLC  
1132 HARDY DRIVE  
COVINGTON, LA 70433

DESIGNED BY: S.BOLLU  
CHECKED BY: S.BOLLU  
SCALE: 36X24  
1"=10'  
DRAWN BY: S.BOLLU  
PROJECT NO.: 2026-101  
DATE: FEB 2026

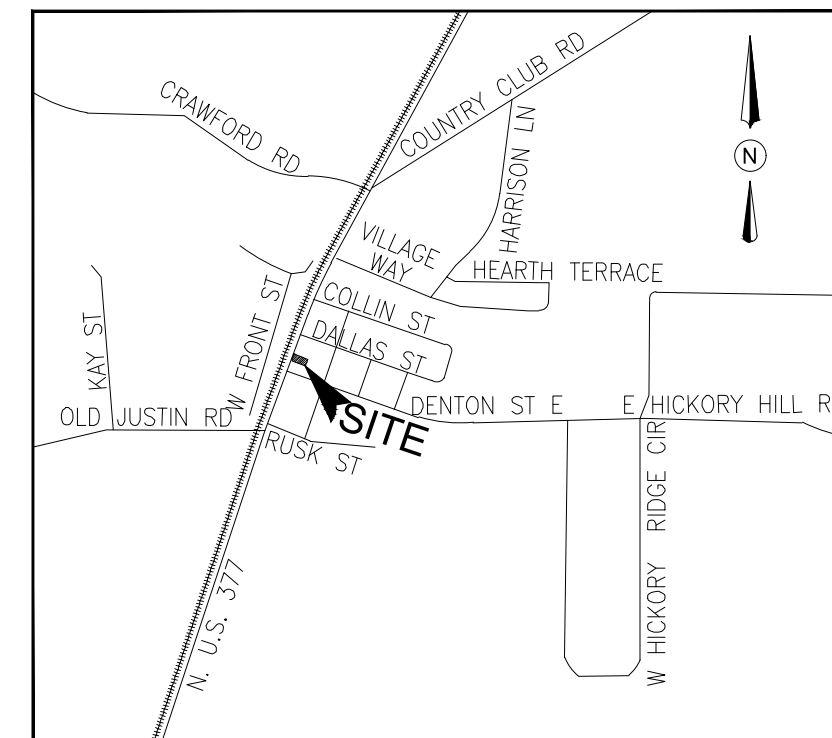
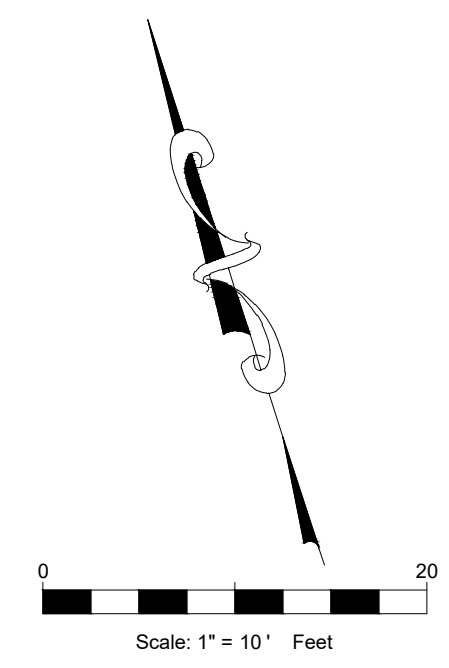
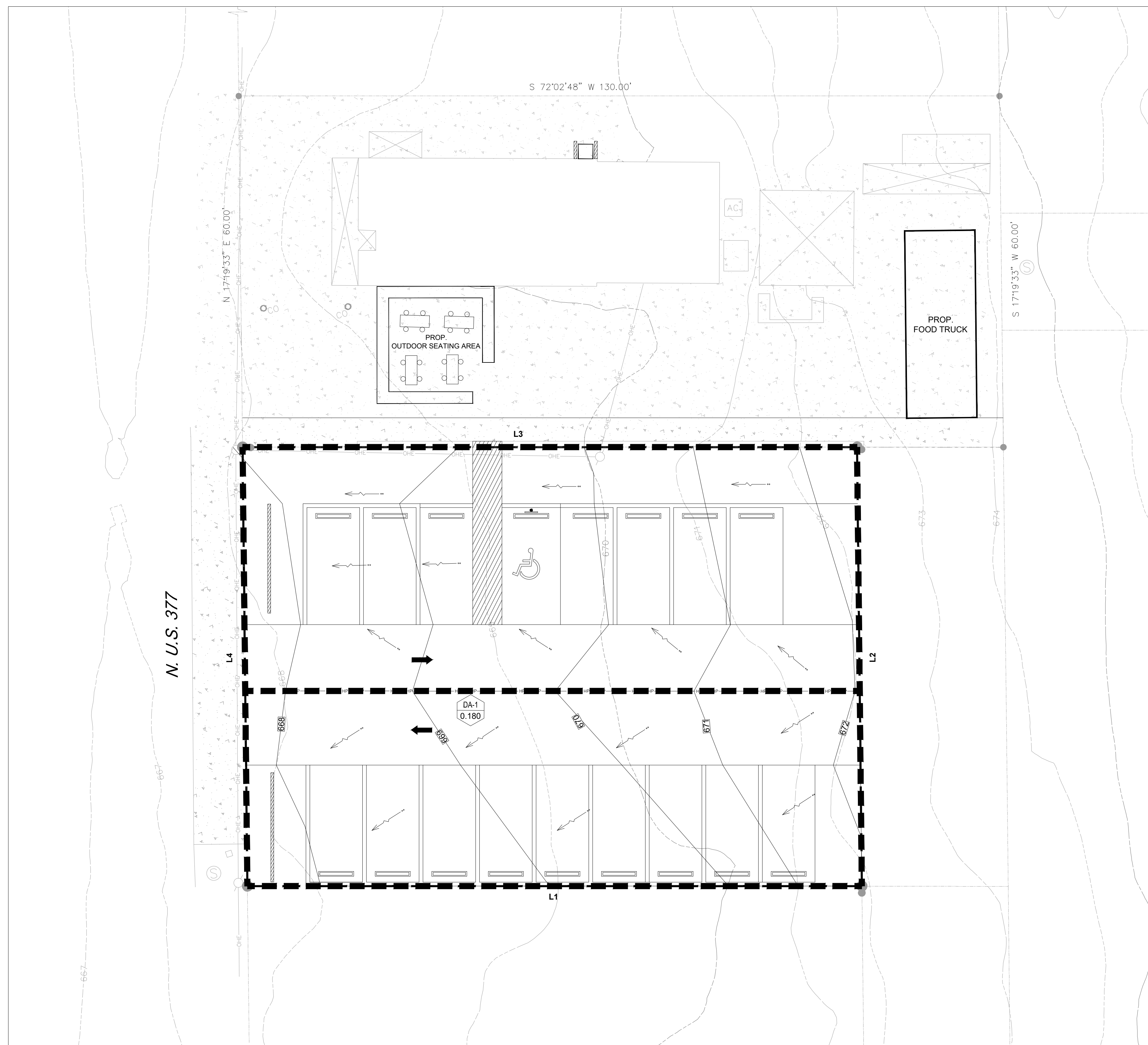
DEMOLITION PLAN  
FOOD TRUCK  
606 N. U.S. 377, ARGYLE  
TEXAS 76226

SHEET NO.  
C-2.0









**EXISTING LEGEND**

- BOUNDARY
- OVER-HEAD-ELECTRIC LINE
- EXISTING MINOR CONTOURS
- EXISTING MAJOR CONTOURS
- ADJECENT BOUNDARY
- CONCRETE PAVEMENT
- CONCRETE
- IRON ROD
- POWER POLE
- WATER METER
- SANITARY SEWER MANHOLE
- VERIZON BOX
- CLEAN OUT
- IRRIGATION CONTROL VALVE

**POST-DRAINAGE LEGEND**

- EXISTING MINOR CONTOURS
- EXISTING MAJOR CONTOURS
- PROPOSED MINOR CONTOURS
- PROPOSED MAJOR CONTOURS
- DRAINAGE DIVIDE
- DRAINAGE AREA NO. (DA-1)
- DRAINAGE AREA ACREAGE (0.180)
- DRAINAGE FLOW DIRECTION

**FLOOD PLAIN NOTE**  
 THIS PROPERTY IS LOCATED WITHIN OTHER AREAS ZONE X. AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP FOR TARRANT COUNTY, TEXAS, COMMUNITY PANEL NUMBER 48121C0510G, MAP EFFECTIVE DATE APRIL 18, 2011.

**PRE DEVELOPMENT DRAINAGE CALCULATIONS**

Drainage Area	C	Tc (min)	I-10 (in/hr)	I-25 (in/hr)	I-50 (in/hr)	I-100 (in/hr)	A (acres)	Q10 Peak (cfs)	Q25 Peak (cfs)	Q50 Peak (cfs)	Q100 Peak (cfs)	Remark
DA-1	0.30	10	6.61	7.8	8.71	9.6	0.180	0.36	0.42	0.47	0.52	Sheet Flow to N. U.S. 377 HIGHWAY
TOTAL							0.180	0.36	0.42	0.47	0.52	

**POST DEVELOPMENT DRAINAGE CALCULATIONS**

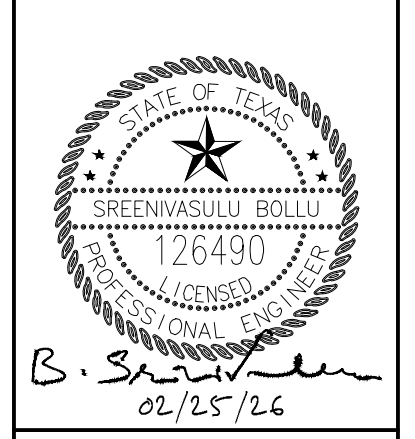
Drainage Area	C	Tc (min)	I-10 (in/hr)	I-25 (in/hr)	I-50 (in/hr)	I-100 (in/hr)	A (acres)	Q10 Peak (cfs)	Q25 Peak (cfs)	Q50 Peak (cfs)	Q100 Peak (cfs)	Remark
DA-1	0.78	10	6.61	7.8	8.71	9.6	0.180	0.93	1.10	1.22	1.35	Sheet Flow to N. U.S. 377 HIGHWAY
TOTAL							0.180	0.93	1.10	1.22	1.35	

Weighted Runoff Calculations:  
 C = (Impervious Area(Asphalt) X 0.9 + Pervious Area(Grass) X 0.3 + Pervious Area(Gravel) X 0.6) / Total Area  
 DA-1: Impervious Area(Asphalt) = 0.137; Pervious Area(Grass) = 0.031; Pervious Area(Gravel) = 0.012  
 C = (0.137 X 0.9 + 0.031 X 0.3 + 0.012 X 0.6) / 0.180 = 0.78

NOTE: INTENSITIES ARE BASED ON ISWM TECHNICAL MANUAL

REVISIONS

NO.	DATE	REVISIONS



**HY ENGINEERING LLC**  
 1132 HARDY DRIVE  
 COVINGTON, LA 70433  
 FIRM #26178  
 SBOLLU@HY-CONSULTING.COM  
 985-951-0413

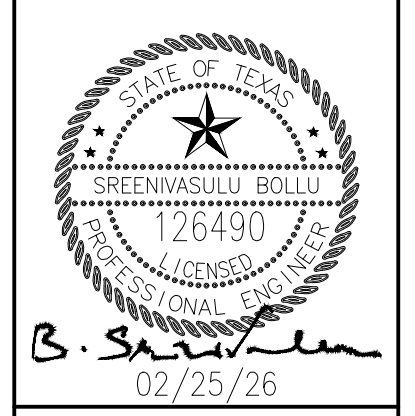
DESIGNED BY: SBOLLU  
 CHECKED BY: SBOLLU  
 DRAWN BY: SBOLLU  
 DATE: FEB 2026  
 PROJECT NO: 2026-101

**DRAINAGE PLAN**  
**FOOD TRUCK**  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226





CADD FILE NAME:	
BASE	
APP'D	
REVISIONS	
REMARKS:	
DATE:	



**HY ENGINEERING LLC**  
 1132 HARDY DRIVE  
 COVINGTON, LA 70433

PROJECT NO. 2026-101  
 DATE: FEB 2026

DESIGNED BY: S.BOLLU  
 CHECKED BY: S.BOLLU  
 DRAWN BY: S.BOLLU

PROJECT NO. 2026-101  
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SBOLLU@HY-CONSULTING.COM  
 FIRM #26178  
 985-951-0413

EROSION CONTROL DETAILS  
 FOOD TRUCK  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226

**1. VEGETATIVE STABILIZATION**  
 This chapter provides standards and specifications for the use of vegetative BMPs for controlling erosion due to land disturbing activities. These methods are primarily for preventing erosion caused by rainfall impact and storm water runoff. The timely use of temporary and permanent ground covers can have a dramatic effect on the amount of erosion that will leave the site of land disturbing activity.  
 Material specifications listed in this Chapter use the latest versions of the North Central Texas Council of Governments, Standard Specifications for Public Works Construction, the Town of Argyle, Special Provisions to Standard Specifications for Public Works Construction and the Texas Department of Transportation, Standard Specifications for Construction of Highways, Streets and Bridges to the maximum extent possible.

**2. TEMPORARY SEEDING**  
 A. Description: The planting of fast growing annual grasses or small grains to provide initial, temporary ground cover for disturbed areas.  
 B. Purpose: To temporarily stabilize disturbed land areas and earthen BMPs that will not be brought to final grade or have permanent stabilization applied within a period of 30 days.  
 C. Applications: This practice applies to graded areas, soil areas with sparse vegetation, and soil areas with no vegetation. Specific construction site applications include diversions, earth dams, temporary sediment basins, roadway embankments, rough graded areas and soil stockpiles.  
 D. Limitations: The application of temporary seeded ground cover has the following limitations:  
 1) Areas must be re-seeded or permanently stabilized within 1-year.  
 2) High cost for short term uses.  
 3) Improper attention to materials and application techniques can lead to higher maintenance costs and severe erosion damage.  
 4) Not applicable to areas used by foot and vehicle traffic.  
 5) Not applicable to areas with excessive stormwater runoff or high velocity runoff.  
 E. Design Criteria: Comply with requirements of NCTCOG Standard Construction Specification Item No. 202, Landscaping.  
 F. Material Specifications: Comply with requirements of NCTCOG Standard Construction Specification Item No. 202, Landscaping.  
 G. Maintenance Requirements: Inspect areas with temporary ground cover weekly and immediately after heavy rainfall events. Repair rills, bare areas, and washouts immediately and re-seed to establish temporary ground cover. Watering may be required to initiate the germination process. The use of annual grasses requires that the areas be re-seeded yearly or be stabilized with permanent ground cover. Protect seeded areas from foot and vehicle traffic.

**3. PERMANENT SEEDING**  
 A. Description: Stabilizing disturbed ground areas by establishing perennial vegetative ground cover by seeding.  
 B. Purpose: To permanently stabilize disturbed areas by establishing a relatively low cost, maintainable ground cover.  
 C. Applications: Permanent vegetative techniques can and should be applied to almost all construction sites at the completion of the project. Permanent seeding is used on fine-graded areas on which long-lived vegetative ground cover is the most practical and effective method for stabilizing the soil. The method can also be used on rough-graded areas that will not be brought to final grade for more than a year.  
 D. Limitations: The application of permanent seeded ground cover has the following limitations:  
 1) Seasonal limits on suitable seeding dates for specific varieties of seed.  
 2) Improper attention to materials and application techniques can lead to higher maintenance costs and severe erosion damage.  
 3) Not applicable to areas used by foot and vehicle traffic.  
 4) Not applicable to areas with excessive stormwater runoff or high velocity runoff.  
 5) Not applicable to steep slopes. Slopes steeper than 3H:1V restrict the use of equipment for seedbed preparation and mowing.  
 6) May require permanent irrigation.  
 E. MATERIAL SPECIFICATIONS: Comply with requirements of the North Central Texas Council of Governments (NCTCOG), Standard Specifications for Public Works Construction, Item 202.5.1 - Solid Sod, along with Item 202.4 - Fertilizer.  
 F. Maintenance Requirements: Repair rills, bare areas, and washouts immediately and re-seed to establish permanent ground cover. Watering, fertilization and soil supplements may be required to establish and maintain permanent ground cover. Remove dead sod and replace promptly. May require periodic mowing and weed control.

**4. REMNANT SODDING**  
 A. Description: Stabilizing bare ground areas by laying a continuous cover of grass sod.  
 B. Purpose: To provide immediate vegetative stabilization to disturbed land areas and earthen BMPs.  
 C. Applications: Practice applies to disturbed land areas that require immediate and permanent ground cover or where sodding is the preferred method of establishing grass. Locations that are particularly well suited to stabilization with sod include:  
 1) Swales, channels and ditches carrying storm water at acceptable velocities.  
 2) Steeper slopes than can be stabilized by normal seeding.  
 3) Residential or commercial lawns and golf courses where prompt use and aesthetics are important.  
 4) Areas around drop inlets after the drainage basin has been stabilized.  
 D. Limitations: The application of sod ground cover has the following limitations:  
 1) More costly to install than seeding.  
 2) More difficult to obtain, transport and store.  
 3) May require permanent irrigation.  
 E. Material Specifications: Comply with requirements of the North Central Council of Governments (NCTCOG), Standard Specifications for Public Works Construction, Item 202.5.1 - Solid Sod, along with Item 202.4 - Fertilizer.  
 F. Maintenance Requirements: Repair rills and washouts immediately and re-sod to establish permanent ground cover. Watering, fertilization and soil supplements may be required to establish and maintain permanent ground cover. Remove dead sod and replace promptly. May require periodic mowing and weed control. Slopes steeper than 3H:1V will require staking of the sod to hold it in place.

**5. OTHER BMPs**  
 Other BMPs shall be included in the design plans.  
 All inlet protection shall meet the requirements of Erosion Control BMPs found in the latest edition of Standard for Construction, North Central Texas Council of Governments (NCTCOG).

APPROVED BY RESOLUTION 2016-35  
 DECEMBER 13, 2016

TOWN OF ARGYLE, TEXAS  
 STANDARD CONSTRUCTION DETAILS  
 EROSION & BMP NOTES  
 DATE: DECEMBER, 2016 SD-38

**4. TEMPORARY STONE CONSTRUCTION ENTRANCE/EXIT**  
 A. Description: An aggregate area or pad located where vehicles enter and leave a construction site.  
 B. Purpose: To provide an area where vehicles can remove mud and sediment from their tires prior to driving on public streets. If used properly, it reduces the requirement to remove sediment from public streets, directs the majority of traffic to a single location, and provides protection for other BMPs on site through traffic control.  
 C. Applications: Use wherever traffic will be leaving a construction site and moving directly onto a public road or an off-site paved surface. Primary installations include exits from storage areas, staging areas, truck haul routes, and borrow/spoil areas.  
 D. Limitations: Selection of the stone construction exit/entrance location is critical, since to be effective all traffic must use the area(s) to exit a site. The device is not suitable for use on long, linear projects unless there are designated points for controlled access. Contractors shall clean-up excessive stone from existing paved streets during the construction process.  
 E. Design Criteria:  
 1) Minimum Pad Dimensions: Width = 15 feet or total width of vehicle access, whichever is greater.  
 Length = 50 feet (Residential lots use 20')  
 Depth = 6 inches  
 2) Grade: Avoid grades steeper than 5% and grade to drain back on to the site or to another BMP to control off-site sedimentation.  
 3) Location: Locate the construction entrance/exit to limit the amount of sediment that leaves the construction site and to provide for maximum use by vehicles leaving the site. Do not place along curves in the public roadway.  
 4) Filter Fabric: Shall be used for installations with a construction period of more than 6 months, where heavy truck traffic is anticipated daily, or very weak sub-grade soils are present.  
 F. Material Specifications  
 1) Aggregate: Natural stone or re-cycled concrete meeting the gradation requirements:  
 • Passing 5-inch Sieve 100%  
 • Retained 2-inch Sieve 100%  
 2) Filter Fabric: NCTCOG Item 201.5.2.1  
 Maintenance Requirements: Inspections should be made weekly and after rain storm events to ensure that the device is functioning properly. When sediment or mud has clogged the void spaces between the stones or mud is being tracked onto the public roadway the aggregate pad must be washed down or replaced. Runoff from the wash-down operation shall not be allowed to drain directly off site without first flowing through another BMP to control off-site sedimentation. Periodic re-grading or the addition of new stone may be required to maintain the efficiency of the installation.

APPROVED BY RESOLUTION 2016-35  
 DECEMBER 13, 2016

TOWN OF ARGYLE, TEXAS  
 STANDARD CONSTRUCTION DETAILS  
 TEMP. STONE CONSTRUCTION ENTRANCE-EXIT  
 DATE: DECEMBER, 2016 SD-25

**6. SILT FENCE**  
 A. Description: A temporary sediment barrier consisting of filter fabric stretched between and attached to metal or wooden posts, with the bottom of the fabric firmly embedded in the soil. At installations draining larger areas the filter fabric will be attached to a hog wire support that is attached to the fence posts.  
 B. Purpose: To slow the flow of sediment laden water from small disturbed areas to allow sedimentation to occur and to filter out larger sediment particles as the water flows through the filter fabric.  
 C. Applications: Silt fence is normally used as a perimeter control immediately downstream of small disturbed areas. It can also be used as a flow diversion for very small drainage areas, but does not function as well as a normal diversion channel and is usually much more expensive.  
 D. Limitations: Do not install silt fences across channels, ditches, streams, pipe outlets, or areas of concentrated water flow. Silt fence locations can limit construction vehicle access so the locations should be well planned to prevent obstructions. Water will pond behind the silt fence, resulting in localized flooding during major rain events.  
 E. Design Criteria: Place silt fence along perimeter of site where disturbed area sheet runoff must be controlled. Limit the drainage area to 0.25 acres per 100 lineal feet of fence. Provide wire support backing whenever the drainage area exceeds 0.10 acres per 100 lineal feet of fence. Maximum post spacing shall not exceed 8 feet. Stone overflow structures or other outlet device shall be installed at all low points along the fence or every 300 feet if there is no apparent low point.  
 F. Material Specifications:  
 1) Filter Fabric: NCTCOG Item 201.5.2.1  
 2) Stone Overflow: NCTCOG Item 201.5.2.4  
 3) Wire Support: NCTCOG Item 201.5.2.3  
 4) Fence Posts: NCTCOG Item 201.5.2.2  
 Maintenance Requirements: Silt fence should be inspected weekly and after major rain events to ensure that the device is functioning properly. Remove sediment from behind fence when the depth of sediment has built up to one-third the height of the fence above grade. Inspect the base of the fence to ensure that no gaps have developed and re-trench as necessary. Inspect fence posts to ensure that they are properly supporting the fence. Straighten, reset and add posts if necessary. If filter fabric is ripped, damaged or deteriorated, replace it in accordance with the original specifications and details.

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 DECEMBER 13, 2016

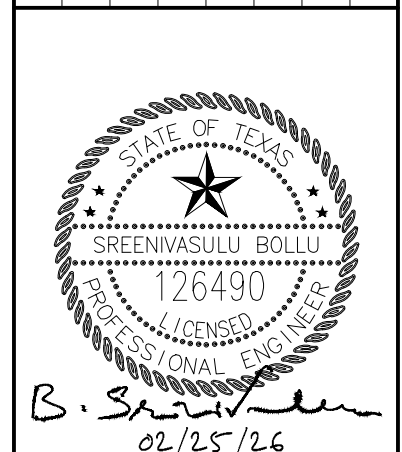
TOWN OF ARGYLE, TEXAS  
 STANDARD CONSTRUCTION DETAILS  
 SILT FENCE - STONE OVERFLOW STRUCTURE  
 DATE: DECEMBER, 2016 SD-27

**CONCRETE WASHOUT AREA DETAIL**  
 N.T.S.

NOTE:  
 CONTRACTOR TO VERIFY WITH TOWN REQUIREMENTS. SHOULD THE TOWN REQUIREMENTS DIFFER FROM DETAIL, THE TOWN REQUIREMENTS WILL SUPERCEDE.

NOTES  
 1. SEE PLAN VIEW FOR CWA INSTALLATION LOCATION.  
 2. DO NOT LOCATE AN UNLINED CWA WITHIN 400' OF ANY NATURAL DRAINAGE PATHWAY OR WATERBODY. DO NOT LOCATE WITHIN 1,000' OF ANY WELLS OR DRINKING WATER SOURCES. IF SITE CONSTRAINTS MAKE THIS WITH INFEASIBLE, OR IF HIGHLY PERMEABLE SOILS EXIST ON SITE, THE CWA MUST BE INSTALLED WITH AN IMPERMEABLE LINER (16 MIL MIN. THICKNESS) OR SURFACE STORAGE ALTERNATIVES USING PREFABRICATED CONCRETE WASHOUT DEVICES OR A LINE ABOVE GROUND STORAGE SHOULD BE USED.  
 3. THE CWA SHALL BE INSTALLED PRIOR TO CONCRETE PLACEMENT ON SITE.  
 4. CWA SHALL INCLUDE FLAT SUBSURFACE PIT THAT IS AT LEAST 8' X 8' SLOPES LEADING OUT OF THE SUBSURFACE PIT SHALL BE 3:1 OR FLATTER. THE PIT SHALL BE AT LEAST 3' DEEP.  
 5. BERM SURROUNDING SIDES AND BACK OF THE CWA SHALL HAVE MINIMUM HEIGHT OF 1'.  
 6. VEHICLE TRACKING PAD SHALL BE SLOPED 2% TOWARDS THE CWA.  
 7. SIGNS SHALL BE PLACED AT THE CONSTRUCTION ENTRANCE, AT THE CWA, AND ELSEWHERE AS NECESSARY TO CLEARLY INDICATE THE LOCATION OF THE CWA TO OPERATORS OF CONCRETE TRUCKS AND PUMP RIGS.  
 8. USE EXCAVATED MATERIAL FOR PERIMETER BERM CONSTRUCTION.  
 9. INSPECT BMPs EACH WORKDAY, AND MAINTAIN IN EFFECTIVE OPERATING CONDITION. MAINTENANCE OF BMPs SHOULD BE PROACTIVE, NOT REACTIVE. INSPECT BMPs AS SOON AS POSSIBLE (AND ALWAYS WITHIN 24 HOURS) FOLLOWING A STORM THAT CAUSES SURFACE EROSION, AND PERFORM NECESSARY MAINTENANCE.  
 10. FREQUENT OBSERVATIONS AND MAINTENANCE ARE NECESSARY TO MAINTAIN BMPs IN EFFECTIVE OPERATING CONDITION. INSPECTIONS AND CORRECTIVE MEASURES SHOULD BE DOCUMENTED THOROUGHLY.  
 11. WHERE BMPs HAVE FAILED, REPAIR OR REPLACEMENT SHOULD BE INITIATED UPON DISCOVERY OF THE FAILURE.  
 12. THE CWA SHALL BE REPAIRED, CLEANED, OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR CONCRETE WASTE. CONCRETE MATERIALS, ACCUMULATED IN PIT, SHALL BE REMOVED ONCE THE MATERIALS HAVE REACHED A DEPTH OF 2'.  
 13. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN SHALL BE TRANSPORTED FROM THE JOB SITE IN A CONTAINER AND DISPOSED OF PROPERLY.  
 14. THE CWA SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.  
 15. WHEN THE CWA IS REMOVED, COVER THE DISTURBED AREA WITH TOP SOIL, SEED AND MULCH OR OTHERWISE STABILIZED IN A MANNER APPROVED BY THE LOCAL JURISDICTION.

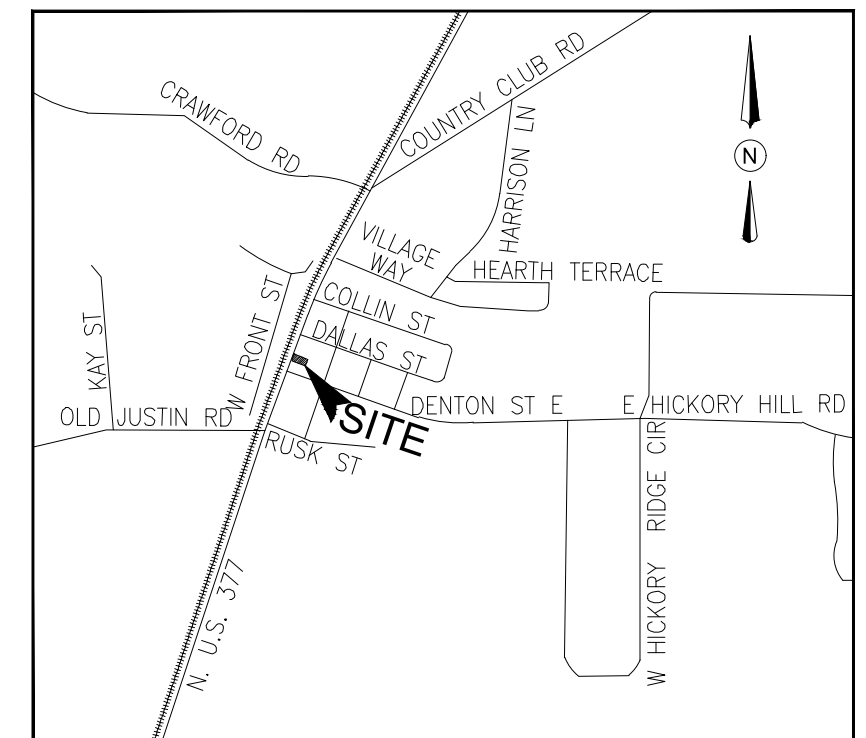
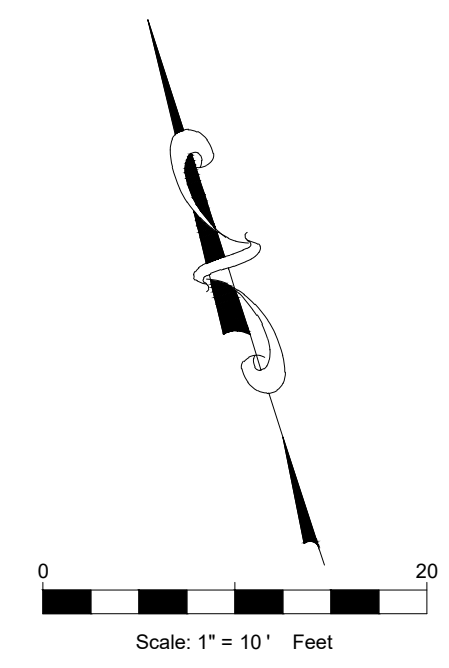
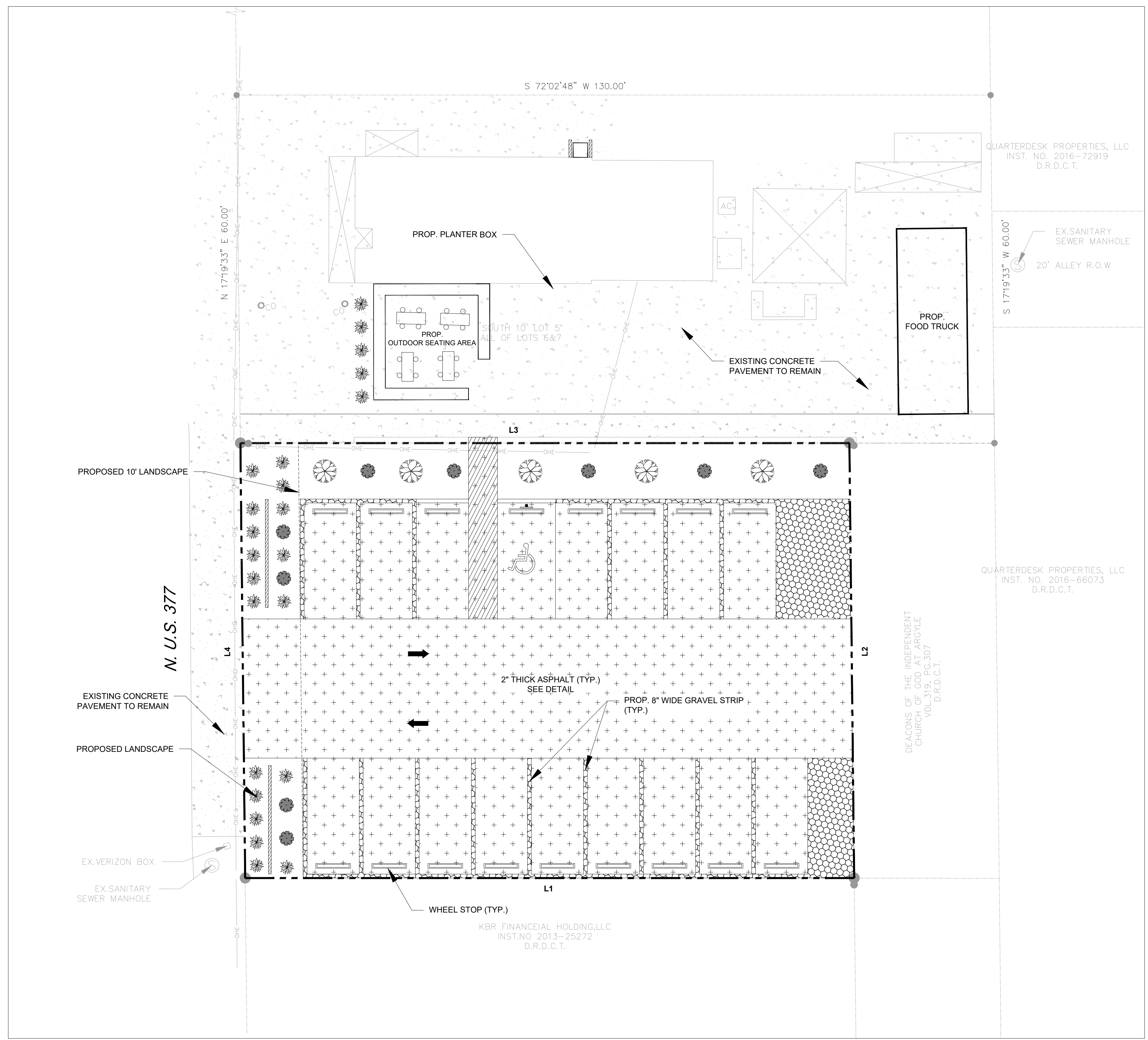
REVISIONS	DATE	REMARKS



**HY ENGINEERING LLC**  
 1132 HARDY DRIVE  
 COVINGTON, LA 70433  
 FIRM #26178  
 SBOLLU@HY-CONSULTING.COM  
 985-951-0413

SCALE: 36X24	DESIGNED BY: SBOLLU
1"=10'	CHECKED BY: SBOLLU
DRAWN BY: SBOLLU	PROJECT NO: 2026-101
DATE: FEB 2026	

**PAVING PLAN**  
**FOOD TRUCK**  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226



EXISTING LEGEND	
---	BOUNDARY
---	OVER-HEAD-ELECTRIC LINE
---	EXISTING MINOR CONTOURS
---	EXISTING MAJOR CONTOURS
---	ADJECENT BOUNDARY
---	CONCRETE PAVEMENT
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○	IRON ROD
○	POWER POLE
○	WATER METER
○	SANITARY SEWER MANHOLE
○	VERIZON BOX
○	CLEAN OUT
○	IRRIGATION CONTROL VALVE

PAVING LEGEND	
+	2" ASPHALT PAVEMENT
○	GRAVEL AREA

- PAVING GENERAL NOTES**
- STRIP & REMOVE FROM THE CONSTRUCTION AREA ALL TOPSOIL, ORGANICS & VEGETATION TO A MINIMUM DEPTH OF 6 INCHES.
  - THE CONTRACTOR SHALL PROCEED WITH PAVING NO MORE THAN SEVENTY-TWO (72) HOURS AFTER DENSITY/MOISTURE TESTS HAVE BEEN TAKEN AND PASSED BY A REGULAR TESTING FIRM.
  - MANHOLE RIM ELEVATIONS, CLEAN-OUTS, VALVE BOXES, ETC. SHALL BE ADJUSTED TO FINISHED GRADE BY THE PAVING CONTRACTOR AT THE TIME OF PAVING.
  - GC TO VERIFY PAVING & SUBGRADE THICKNESS WITH GEO TECH REPORT AND FOLLOW GEO TECH RECOMMENDATIONS.

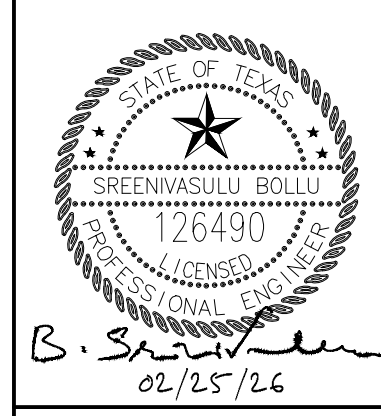
PROPOSED FLEXIBLE PAVEMENT RECOMMENDATIONS				
PAVEMENT THICKNESS & STRENGTH	LEGEND	BASE COURSE RECOMMENDATIONS	SUBGRADE RECOMMENDATIONS	PROPOSED USE
2" ASPHALTIC SURFACE COURSE		8" BASE COURSE (CRUSHED CONCRETE / LIME ROCK)	12" STABILIZED SUBGRADE (LBR >40)	FIRE LANE, DRIVE AISLE AND PARKING LOTS

NOTE: PAVEMENT THICKNESS SHOWN HERE IS FOR REFERENCE ONLY. GEOTECHNICAL ANALYSIS IS REQUIRED AND REFER GEOTECH REPORT FOR RECOMMENDATIONS AND DETAILS





REVISED	DATE	REMARKS

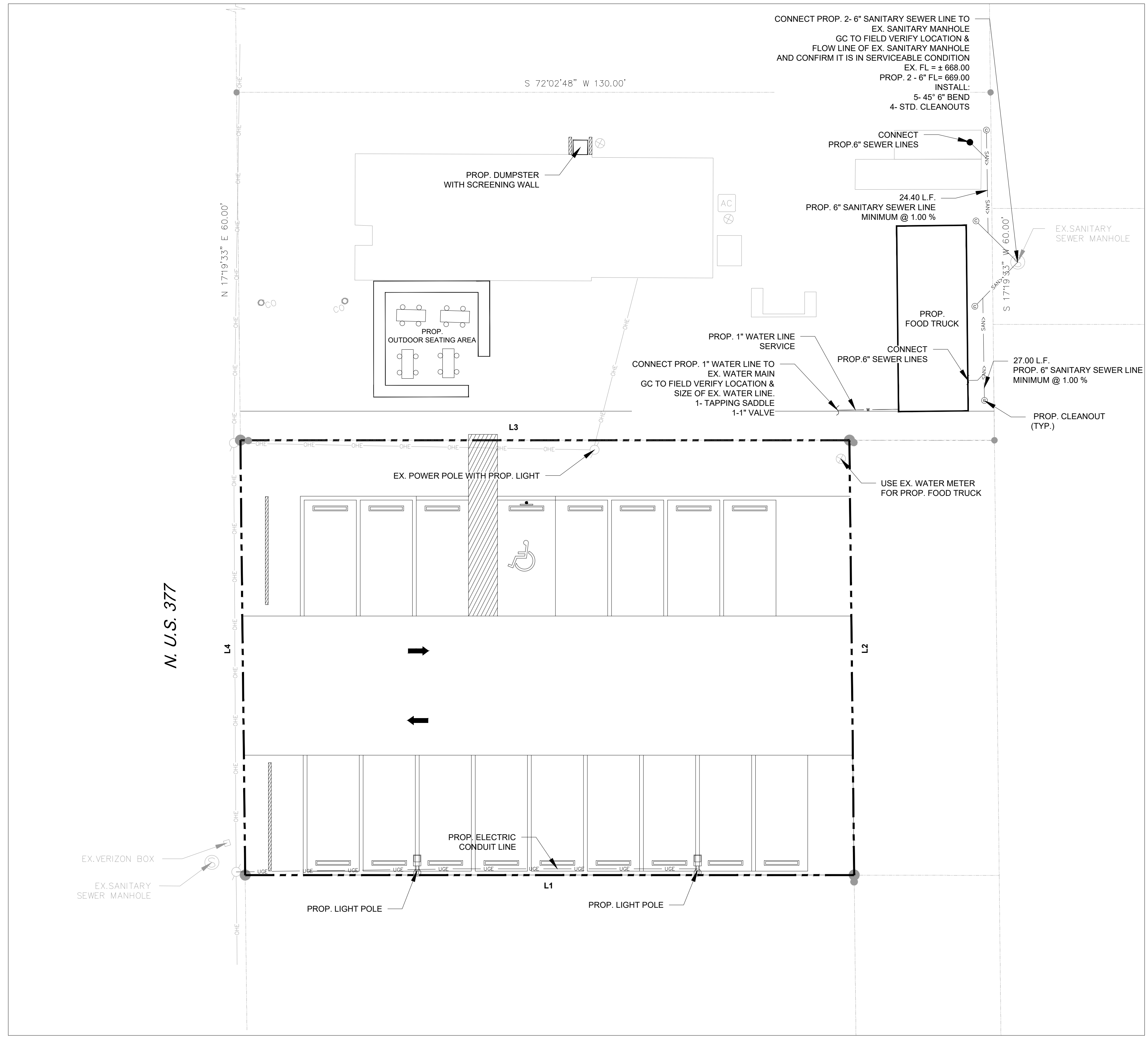


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 COVINGTON, LA 70433

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SCALE:	36X24
DATE:	FEB 2026
DRAWN BY:	S.BOLLU
DATE:	FEB 2026

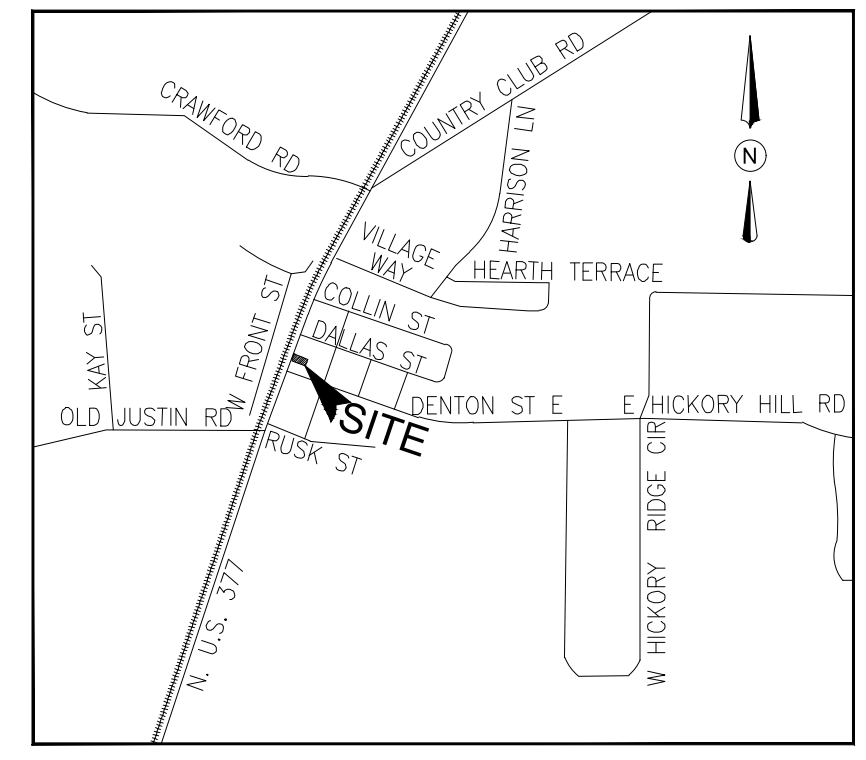
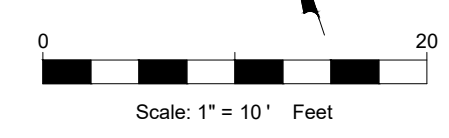
**UTILITY PLAN**  
**FOOD TRUCK**  
 606 N. U.S. 377, ARGYLE  
 TEXAS 76226



EXISTING LEGEND	
---	BOUNDARY
---	OVER-HEAD-ELECTRIC LINE
---	EXISTING MINOR CONTOURS
---	EXISTING MAJOR CONTOURS
---	ADJECENT BOUNDARY
---	CONCRETE PAVEMENT
---	CONCRETE
○	IRON ROD
○	POWER POLE
○	WATER METER
○	SANITARY SEWER MANHOLE
○	VERIZON BOX
○	CLEAN OUT
○	IRRIGATION CONTROL VALVE

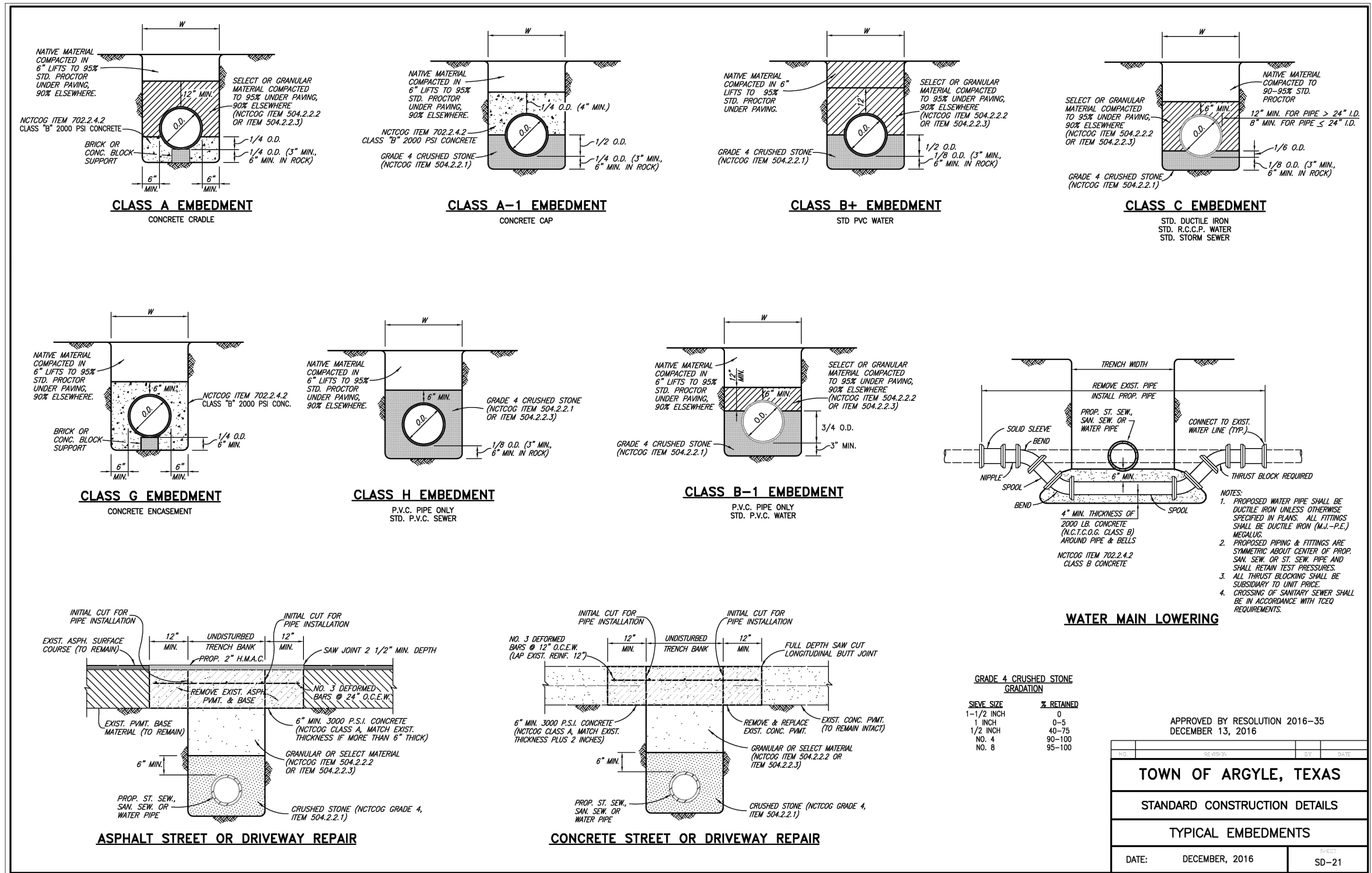
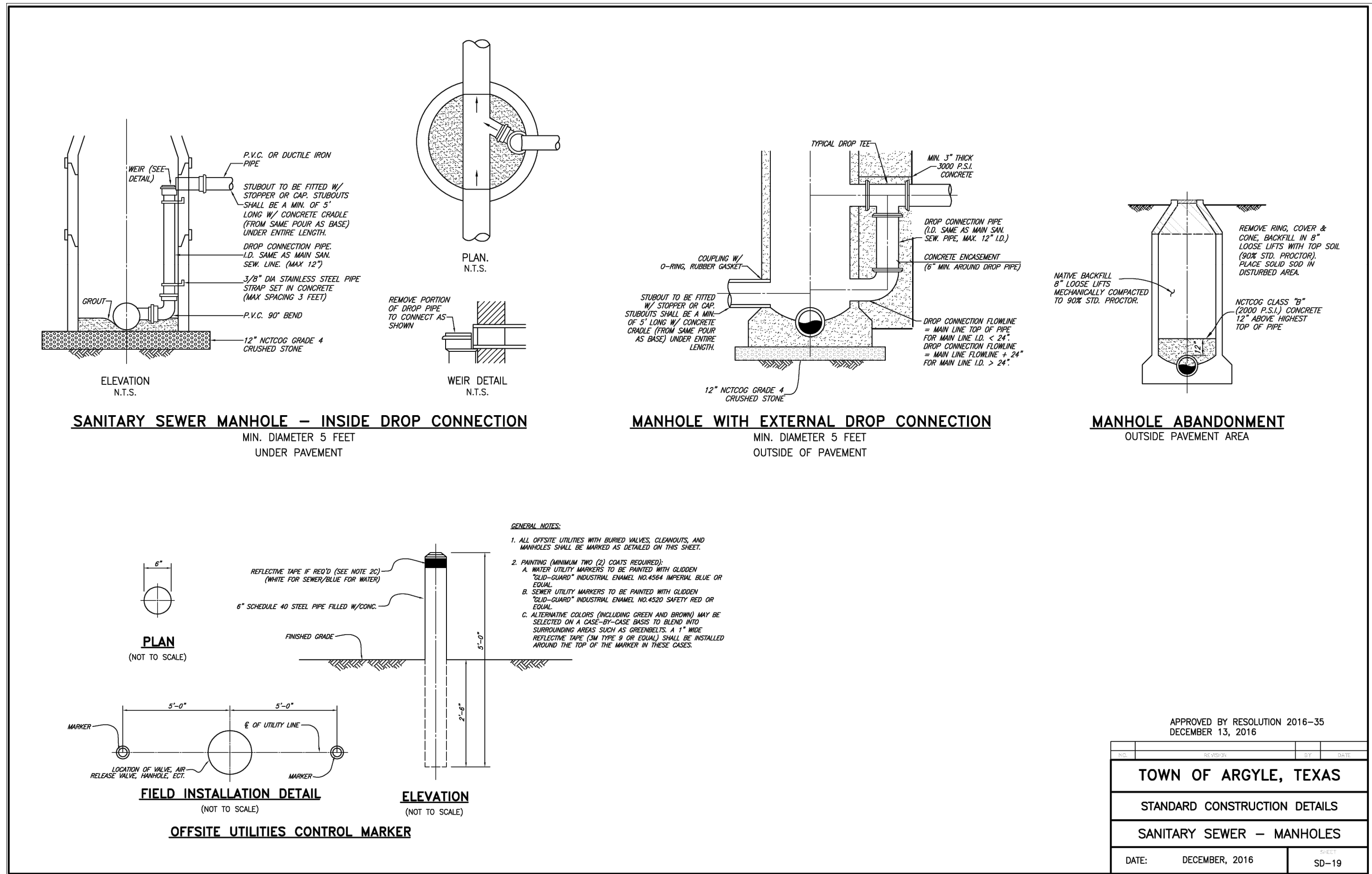
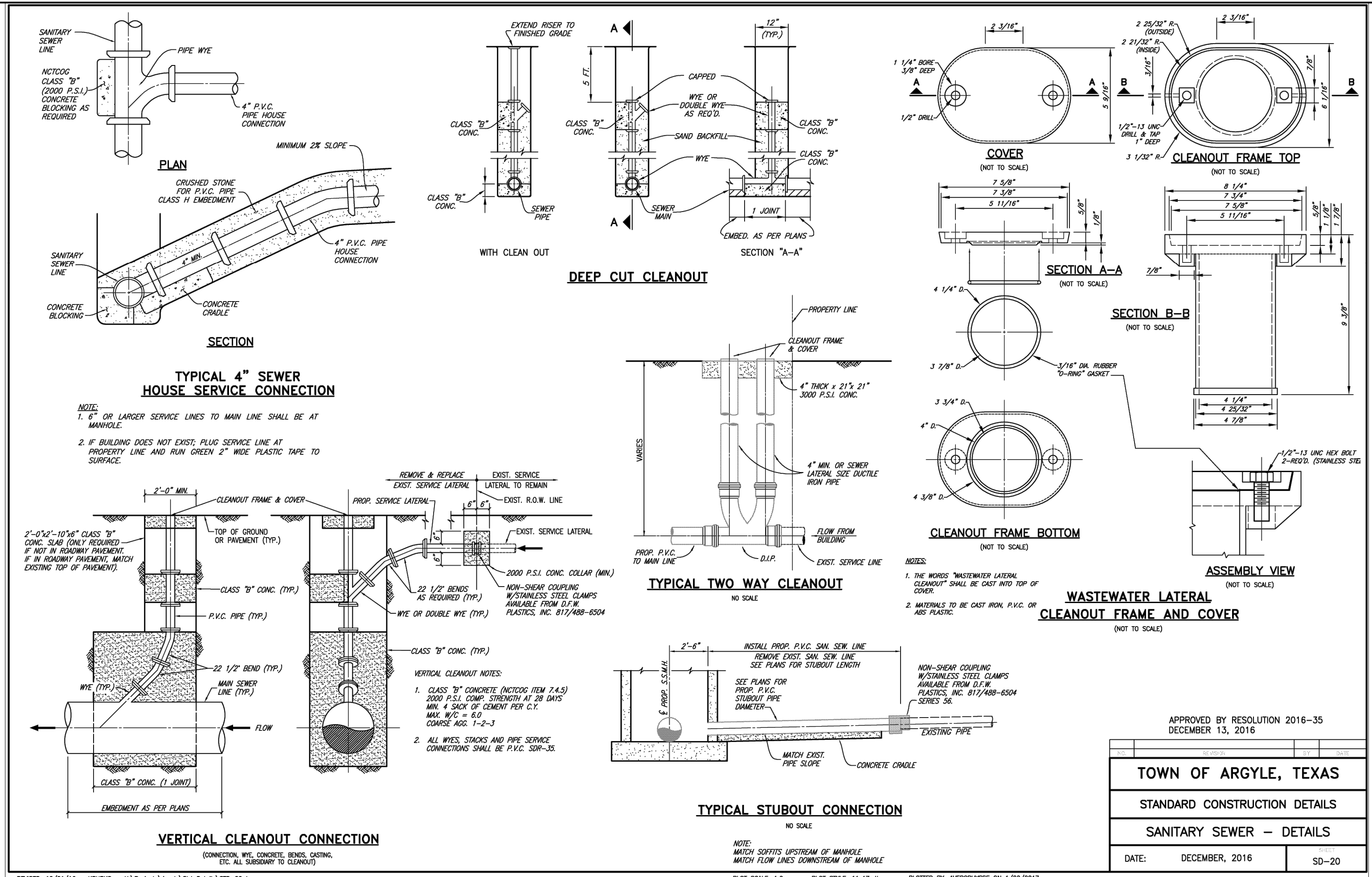
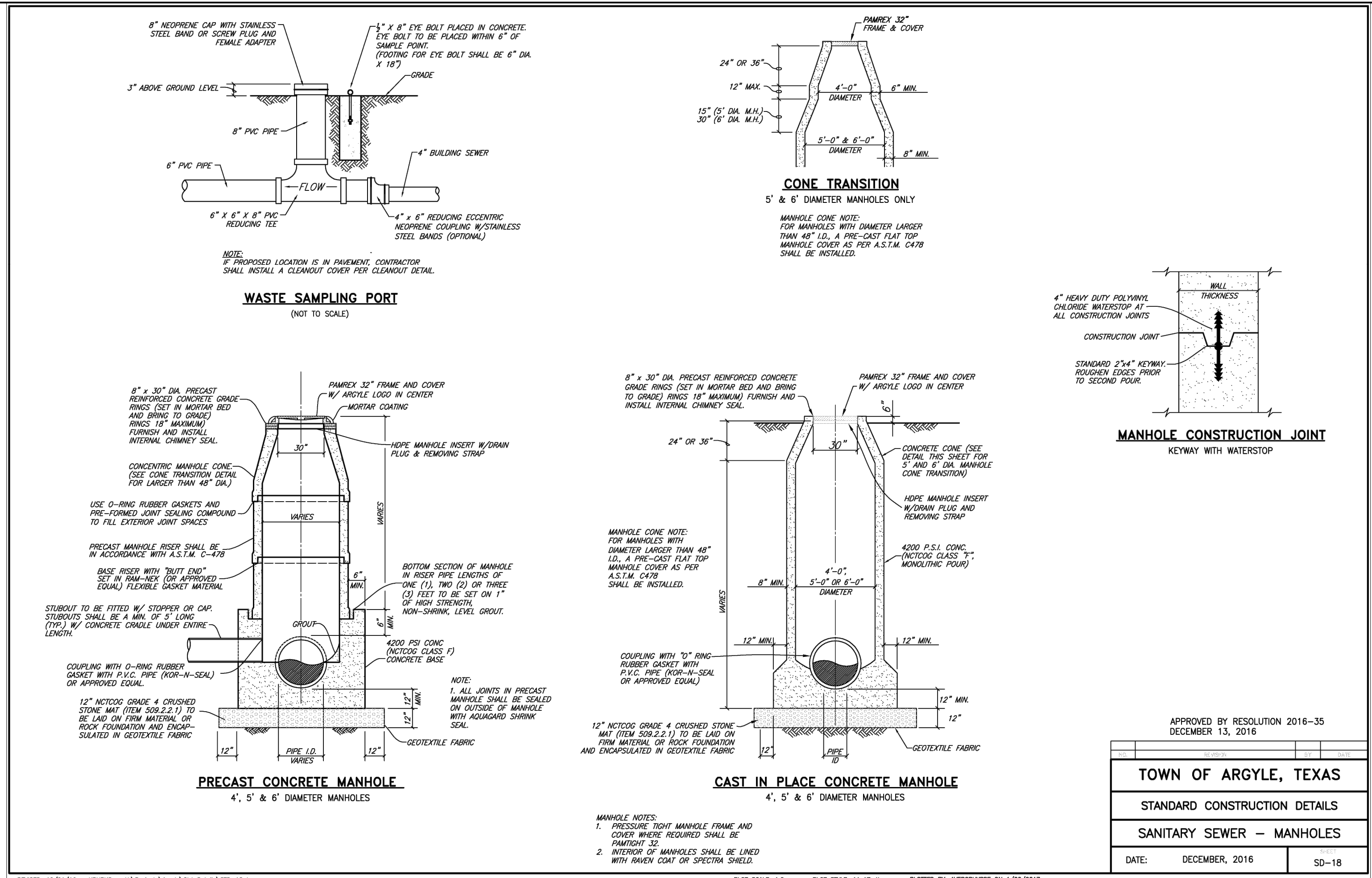
UTILITY LEGEND	
---	SANITARY SEWER LINE
---	WATER LINE
○	SANITARY SEWER MANHOLE
○	SANITARY SEWER CLEANOUT
○	DOMESTIC WATER METER

- UTILITY GENERAL NOTES**
- ALL CONSTRUCTION SHALL BE IN STRICT ACCORDANCE WITH THE TOWN/UTILITY COMPANY STANDARDS.
  - FIELD VERIFY LOCATION OF EXISTING WATER MAIN, SEWER MAIN, GAS, TELEPHONE AND ELECTRICAL LINE. POT HOLE RECOMMENDED PRIOR TO CONSTRUCTION BEGIN. CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE WITH UTILITY SERVICE PROVIDERS.
  - THE LOCATION OF UNDERGROUND UTILITIES INDICATED ON THE PLANS IS TAKEN FROM AS-BUILTS, UTILITY PLANS OR SURVEY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND UTILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL UTILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND UTILITIES. IF EXISTING UNDERGROUND UTILITIES ARE DAMAGED, THE CONTRACTOR WILL BE RESPONSIBLE FOR THE COST OF REPAIRING THE UTILITY.
  - WHERE EXISTING UTILITIES OR SERVICE LINES ARE CUT, BROKEN OR DAMAGED, THE CONTRACTOR SHALL REPLACE OR REPAIR THE UTILITIES OR SERVICE LINES WITH THE SAME TYPE OF ORIGINAL MATERIAL AND CONSTRUCTION, OR BETTER, UNLESS OTHERWISE SHOWN OR NOTED ON THE PLANS, AT HIS OWN COST AND EXPENSE. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER AT ONCE OF ANY CONFLICTS WITH UTILITIES.
  - ALL EXCAVATIONS, TRENCHING AND SHORING OPERATIONS SHALL COMPLY WITH THE REQUIREMENTS OF THE U. S. DEPARTMENT OF LABOR, OSHA, CONSTRUCTION SAFETY AND HEALTH REGULATIONS AND ANY AMENDMENTS THERETO.
  - THE CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED BY CONSTRUCTION TO ORIGINAL CONDITION OR BETTER. RESTORED AREAS INCLUDE, BUT ARE NOT LIMITED TO TRENCH BACKFILL, SIDE SLOPES, FENCES, CULVERT PIPES, DRAINAGE DITCHES, DRIVEWAYS, PRIVATE YARDS AND ROADWAYS.



VICINITY MAP N.T.S.





CADD FILE NAME: Item 8

BASE

REVISIONS

REMARKS:

DATE:

STATE OF TEXAS  
SREENIVASULU BOLLU  
126490  
LICENSED PROFESSIONAL ENGINEER  
B. Sreenivasulu  
02/25/26

HY ENGINEERING LLC  
1132 HARDY DRIVE  
COVINGTON, LA 70433

FIRM #26178  
SBOLLU@HY-CONSULTING.COM

DESIGNED BY: S.BOLLU  
CHECKED BY: S.BOLLU  
DRAWN BY: S.BOLLU  
DATE: FEB 2026

PROJECT NO: 2026-101  
985-951-0413

UTILITY DETAILS  
FOOD TRUCK  
606 N. U.S. 377, ARGYLE  
TEXAS 76226

SCALE: 3/8"=1'-0"

DESIGNED BY: S.BOLLU  
CHECKED BY: S.BOLLU  
DRAWN BY: S.BOLLU  
DATE: FEB 2026

PROJECT NO: 2026-101  
985-951-0413

APPROVED BY RESOLUTION 2016-35  
DECEMBER 13, 2016

TOWN OF ARGYLE, TEXAS	
STANDARD CONSTRUCTION DETAILS	
TYPICAL EMBEDMENTS	
DATE:	DECEMBER, 2016
SD-	21





# EXHIBIT B







Made with Love  







# EXHIBIT C

